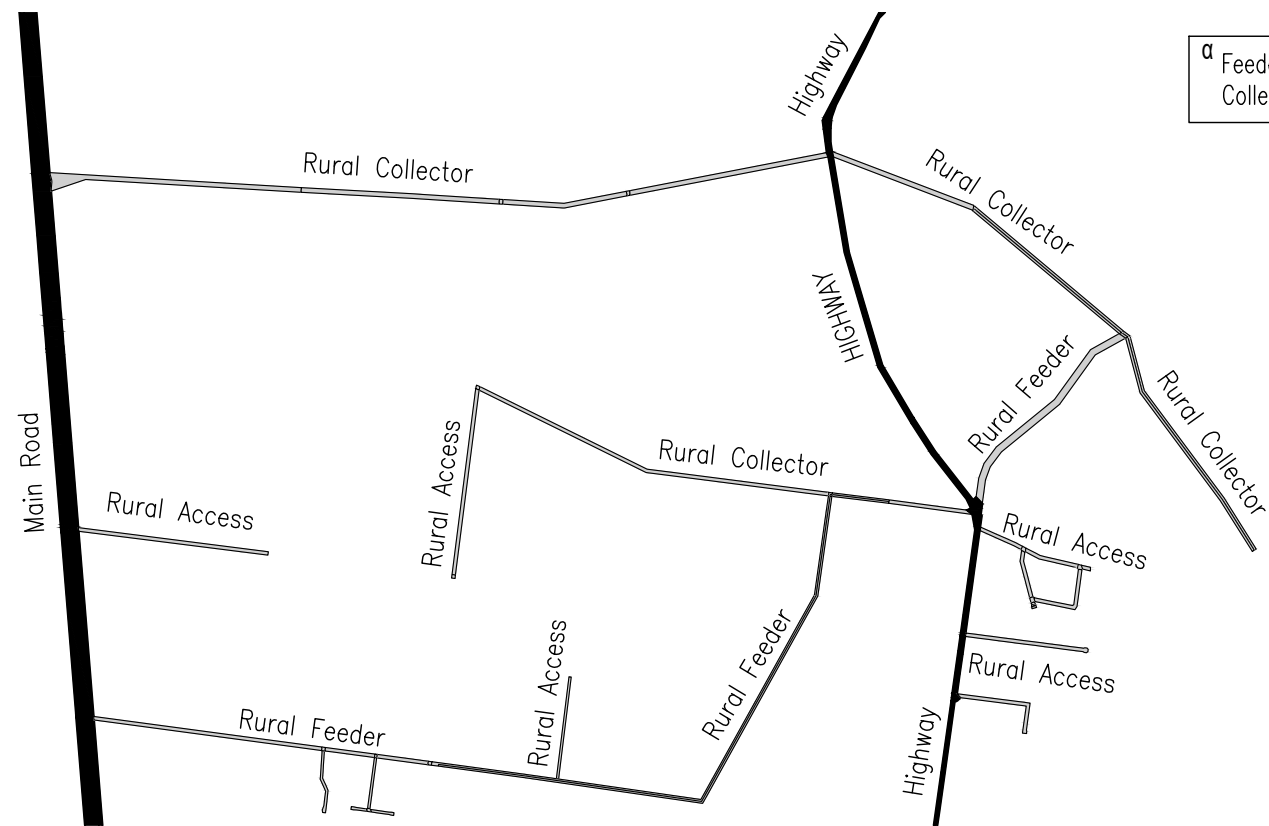


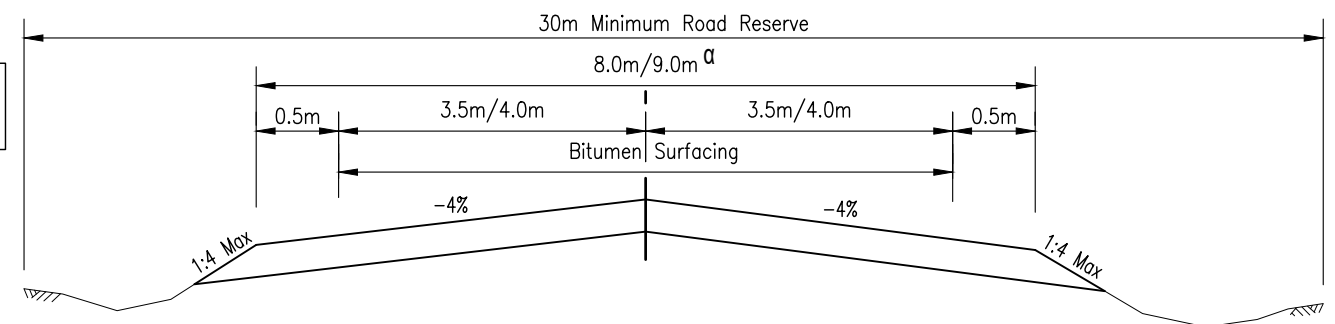
STD. DWG. NO.				DESCRIPTIONS					STD. DWG. NO.	DESCRIPTIONS											
CROSS SECTIONS										OTHER											
R-002				TYPICAL RURAL ROAD CROSS SECTIONS					R-017	MINIMUM TURNING TREATMENTS – FOR RURAL ROADS											
R-003				TYPICAL URBAN ROAD CROSS SECTIONS					R-018	TYPICAL CULVERT AND SLAB DETAIL											
				DRIVEWAYS					R-019	TYPICAL CONCRETE FOOTPATH DETAILS											
R-004				RURAL & RESIDENTIAL TURNOUTS – GRAVEL INVERTS AND CONCRETE PIPE/CULVERT TURNOUTS																	
R-005				URBAN CONCRETE DRIVEWAYS																	
R-006				COMMERCIAL DRIVEWAY SLAB																	
R-007				COMMERCIAL AND/OR TRUCK TURNOUT																	
KERBING																					
R-008				KERBING PREPARATION DETAILS																	
R-009				KERB RAMP																	
R-016				ROOFWATER DRAINAGE KERB ADAPTOR																	
OTHER																					
R-010				TYPE CROSS SECTION FOR CONCRETE FOOTPATH TYPES AND JOINING DETAILS AND TELSTRA PIT JOINING DETAILS																	
R-011				STREET SIGNS AND LOCATION OF STREET SIGNS																	
R-012				CONCRETE MACHINERY CROSSING																	
R-013				INVERT DRAIN AT ROAD INTERSECTION																	
R-014				TYPICAL ROADS WITH SERVICE ALIGNMENTS																	
R-015				TYPICAL CONCRETE FLOODWAY																	
R-016				ROOFWATER DRAINAGE KERB ADAPTORS FOR BARRIER KERB AND MOUNTABLE KERB																	
				Field Book No.	DRAWN S. Robertson			<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>		Horiz. Section		STANDARD DRAWING – ROADS INDEX									
				Level Book No.	DESIGNED S. Forbes									NOT TO SCALE							
				Datum	CHECKED S. Forbes																
					EXAMINED G. Parsons									Vert. Section							
					RECOMMENDED S. Hegedus RPEQ.									NOT TO SCALE							
					TECHNICAL SERVICES MANAGER																
					DATE 16/07/2010																
A	Original Issue							Job No./s				Works Order No.		Auxiliary Plan No's.							
Revisions		Drn by	Date											Plan No. R-001							
														No. 1 of 16Plans							
														Rev. A							

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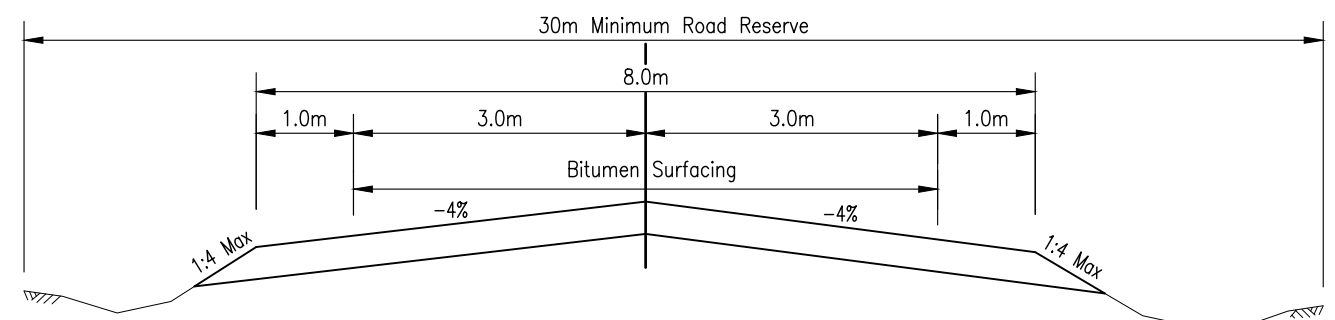


**ROAD HIERARCHY**

$\alpha$ Feeder	8.0m
Collector	9.0m

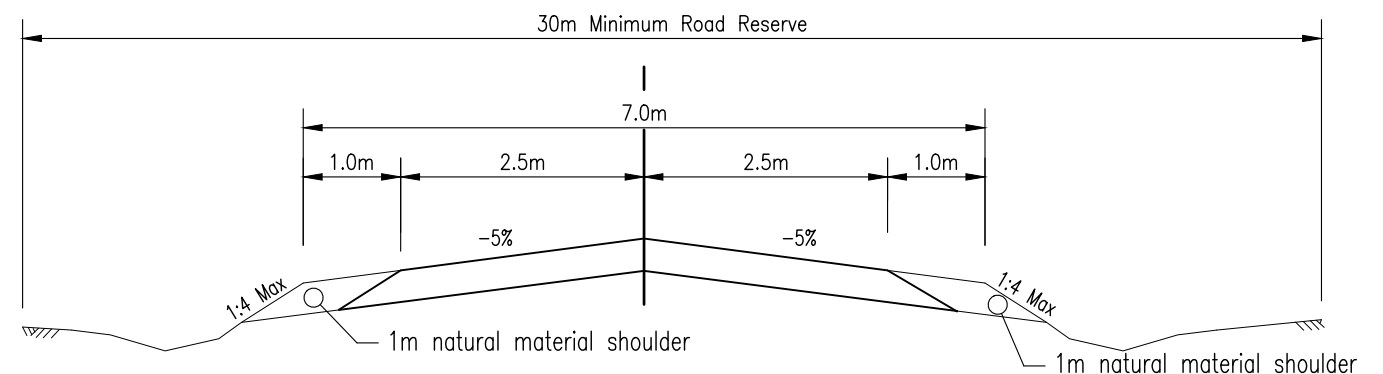


**RURAL FEEDER/COLLECTOR**

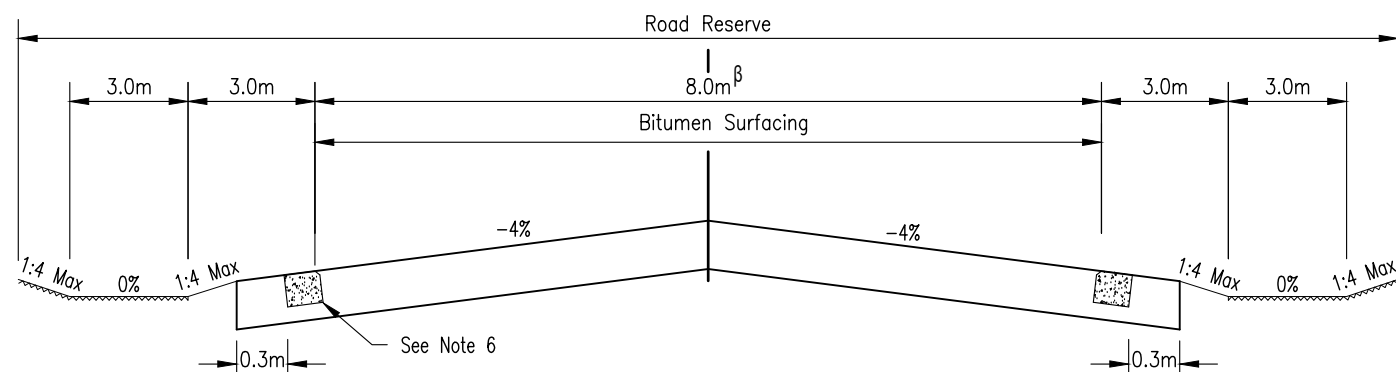


**RURAL ACCESS 1**

Full bitumen surfacing for rural residential roads to be 8.0m

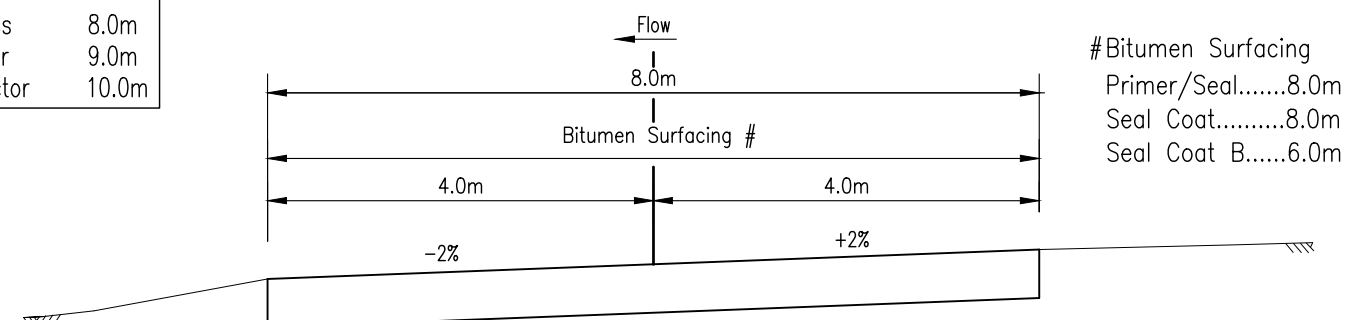


**RURAL ACCESS 2 (NIL BITUMEN)**



**RURAL-RESIDENTIAL ROAD**

$\beta$ Access	8.0m
Feeder	9.0m
Collector	10.0m



**FLOODWAYS**

#Bitumen Surfacing  
Primer/Seal.....8.0m  
Seal Coat.....8.0m  
Seal Coat B.....6.0m

<b>F</b>	Geofabric seal removed	L.T.P.	28.10.14	<b>Field Book No.</b>	<b>DRAWN</b> L. Porter		Horiz. Section NOT TO SCALE  Vert. Section NOT TO SCALE	<b>STANDARD DRAWING – ROADS</b> <b>TYPICAL RURAL ROAD CROSS SECTIONS</b>
<b>E</b>	Clarification of widths	S.E.R.	16.05.13	<b>Level Book No.</b>	<b>DESIGNED</b> S. Forbes			
<b>D</b>	Rural-Residential added	L.T.P.	14.02.12	<b>Datum</b>	<b>CHECKED</b> S. Forbes			
<b>C</b>	Change to Rural Collector	L.T.P.	30.02.11		<b>EXAMINED</b> G. Parsons			
<b>B</b>	Note on Rural Access 1	L.T.P.	21.02.11		<b>RECOMMENDED</b> S. Hegedus RPEQ.			
<b>A</b>	Original Issue				<b>TECHNICAL SERVICES MANAGER</b>			
					DATE 13/07/2010			
					Job No./s	Works Order No.	Auxiliary Plan No's.	
					Revisions	Drn by	Date	Plan No. R-002 No. 2 of 16 Plans Rev. F

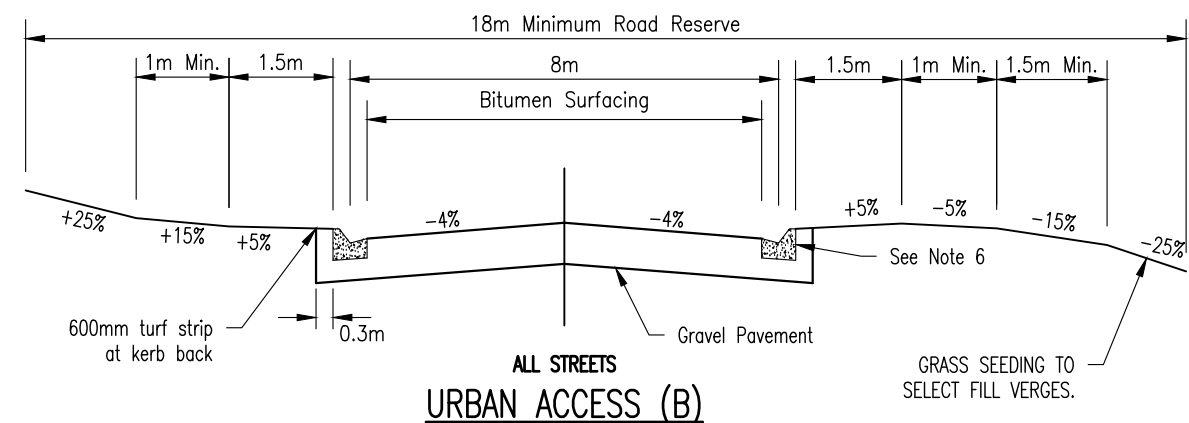
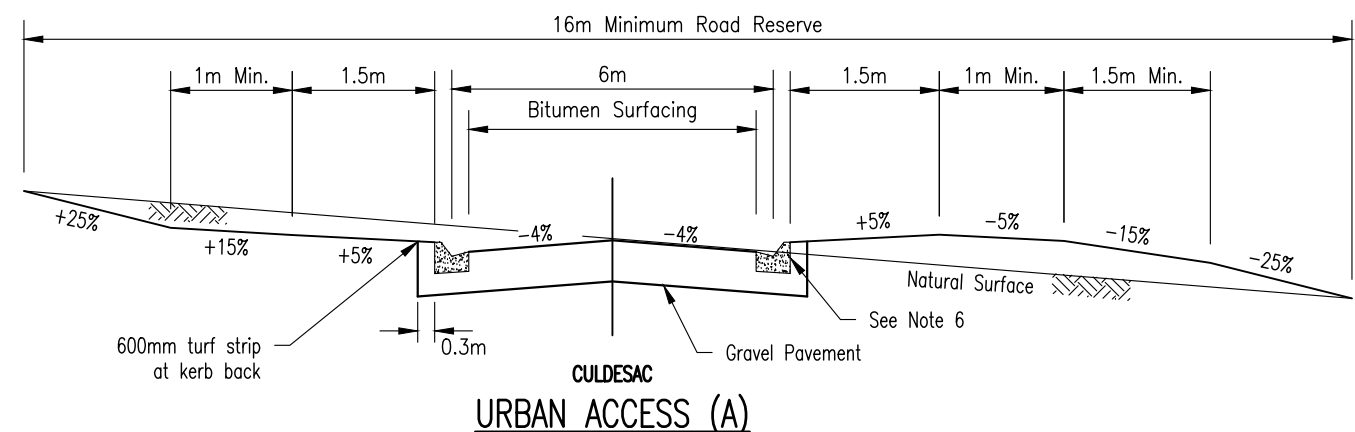
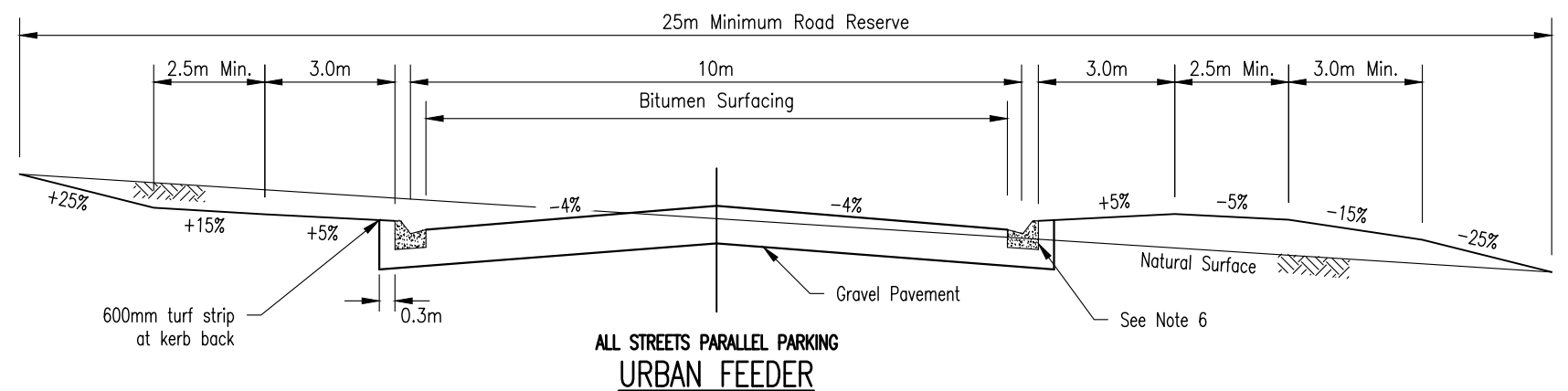
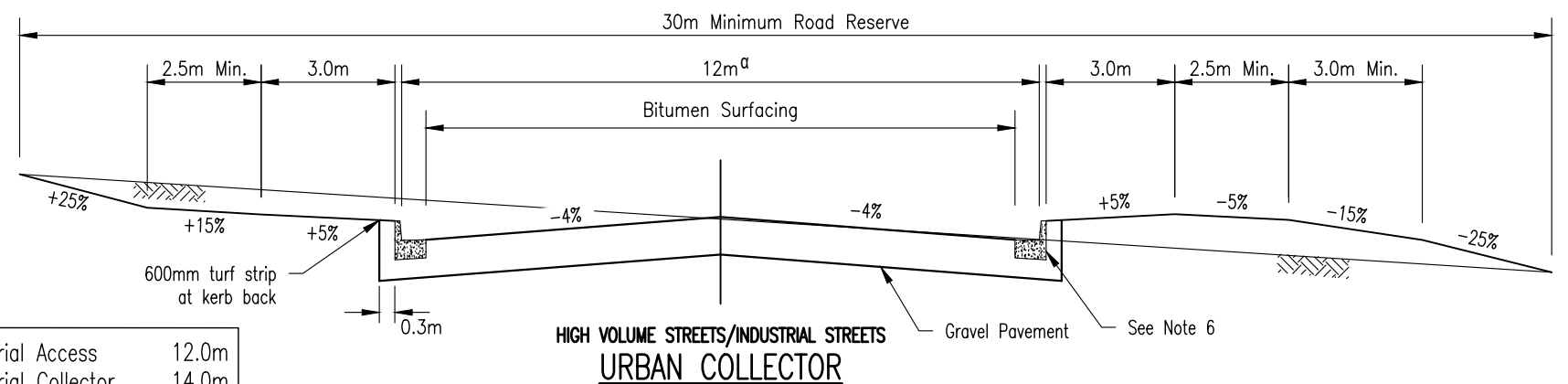


**ROAD HIERARCHY**

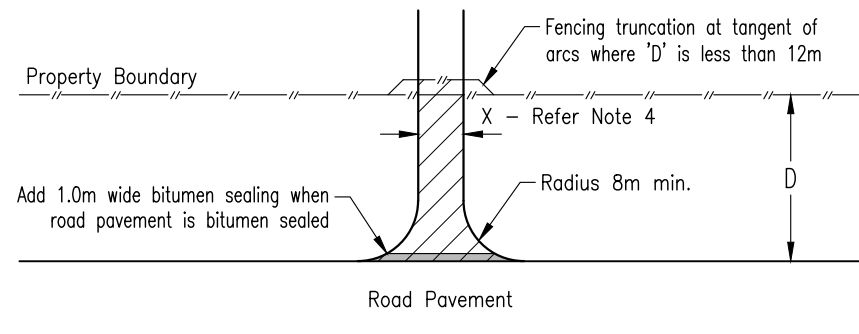
**NOTES**

1. See Western Downs Regional Council Standard Drawing R-002 for Rural Road cross sections and Hierarchy Plan.
2. Dimensions of verge/footpath may vary according to traffic composition, speed environment, topography etc.
3. Pavement depths shall be designed to suit the subgrade conditions, traffic volume and quality of available pavement.
4. Designs are to be approved by WDRC's Technical Services Manager as part of the Operational works permit.
5. Bitumen surfacing to be designed to the satisfaction of WDRC's Technical Services Manager or WDRC's Development Engineer.
6. Kerb and channel installation is subject to underground drainage provisions and must be laid at 0.4% min grade. See Western Downs Regional Council Standard Drawing R-008 for edge beam detail.
7. Angle parking provisions will be conditions of certain developments. These typical cross sections display minimum carriageway widths and slopes only.
8. Concrete bikeways/footpaths will be conditions of certain developments. Western Downs Regional Council maintains a Master Plan of the bikeways/footpaths network.
9. Refer to Western Downs Regional Council Standard Drawing R-014 for positions of services.

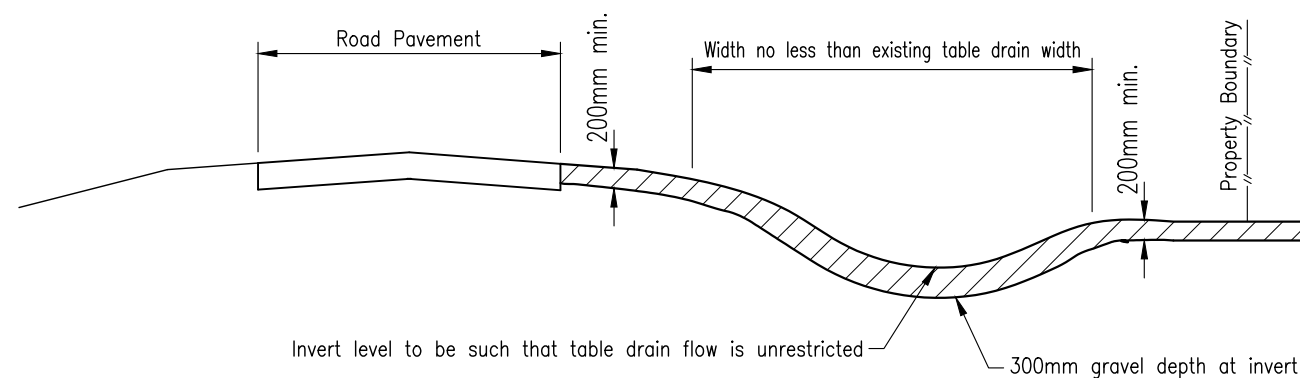
<sup>a</sup> Industrial Access	12.0m
Industrial Collector	14.0m



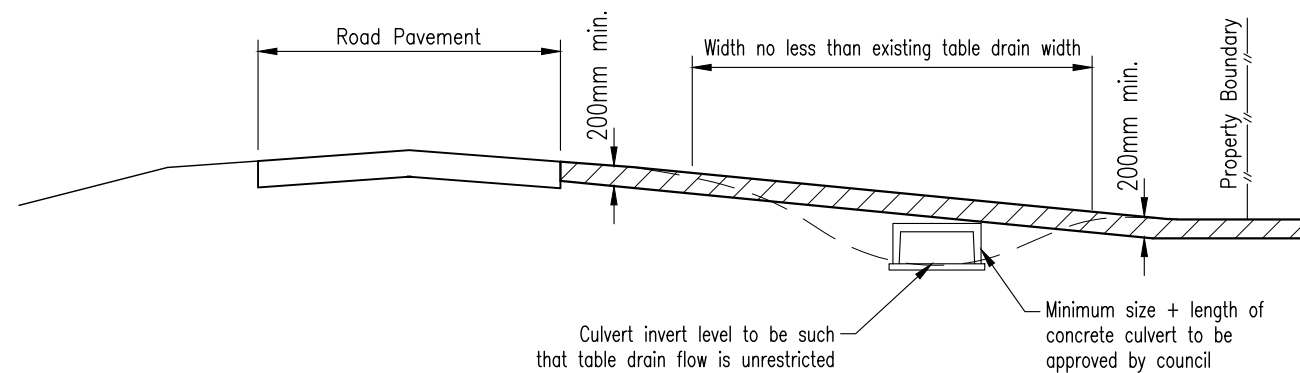
				Field Book No.	DRAWN L. Porter		<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>	Horiz. Section NOT TO SCALE  Vert. Section NOT TO SCALE	STANDARD DRAWING – ROADS TYPICAL URBAN ROAD CROSS SECTIONS
E	Geofabric seal removed	L.T.P.	28.10.14	Level Book No.	DESIGNED S. Forbes				
D	Clarification of widths	S.E.R.	16.05.13	Datum	CHECKED S. Forbes				
C	Geofabric seal added	L.T.P.	14.02.12		EXAMINED G. Parsons				
B	Drain changed on all XS	L.T.P.	17.11.11		RECOMMENDED S. Hegedus RPEQ.				
A	Original Issue				TECHNICAL SERVICES MANAGER				
					DATE 15/07/2010				
Revisions		Drn by	Date		Job No./s	Works Order No.	Auxiliary Plan No's.		



TURNOUT DETAIL – RESIDENTIAL



GRAVEL INVERT SECTION



CONCRETE PIPE SECTION

#### NOTES

1. Dimensions of turnout and pavement depths shall not be less than those shown above unless otherwise approved by Council's Technical Services Manager.
2. Quality of pavement material for turnout to be approved by Council's Technical Services Manager.
3. Invert level at turnout to be such that flow in table drain is not restricted in any way.
4. Minimum width of access (shown marked X) 3.0m light traffic only, 4.0m normal traffic. minimum radius on turnout 8.0m.
5. Persons carrying out work on their own behalf, with Council permission, shall be responsible for the safety of road users and would be well advised to have public risk insurance.
6. Acceptance of permission from Council to construct a private turnout shall indemnify the council from any claim or demand whatsoever.
7. All culvert work shall be carried out in accordance with Department of Transport and Main Roads Standard Drawings 1250 & 1260. If precast slabs, endwalls or aprons are approved then all work shall be carried out to the manufacturer's specifications and to the satisfaction of Council's Technical Services Manager.
8. For Commercial and/or Truck Turnout, refer to Standard Drawing R-007.

				Field Book No.	DRAWN L. Porter	<b>WESTERN DOWNS</b> REGIONAL COUNCIL 	Horiz. Section	STANDARD DRAWING – ROADS RURAL & RESIDENTIAL TURNOUTS GRAVEL INVERTS AND CONCRETE CULVERT TURNOUTS
				Level Book No.	DESIGNED S. Forbes		NOT TO SCALE	
D	Remove pipe from detail	J.R.A	22.04.22	Datum	CHECKED S. Forbes		Vert. Section	
C	Notation updated	J.L.R.	16.09.13		EXAMINED G. Parsons		NOT TO SCALE	
B	Notes updated	L.T.P.	31.08.11		RECOMMENDED S. Hegedus RPEQ.			
A	Original Issue				TECHNICAL SERVICES MANAGER			
Revisions				Drn by	Date	Job No./s	Works Order No.	Auxiliary Plan No's.
								Plan No.R-004 No. 4 of 16Plans Rev.D

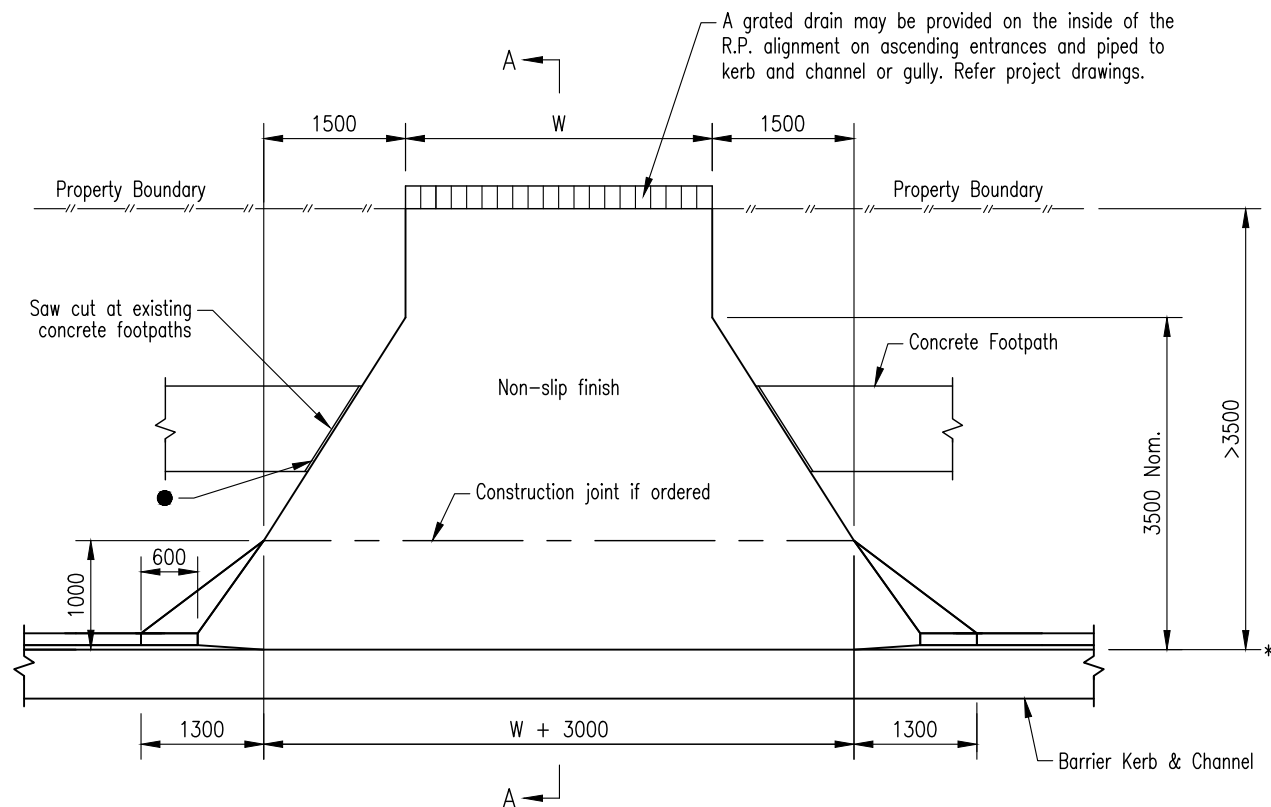


1. Crossings are not designed for commercial vehicles.
2. Footpath section to vary where necessary to match concrete footpaths and verge profiles.
3. Concrete N32 in accordance with AS 1379 and AS 3600.
4. Reinforcement fabric to AS 4671, 50mm top and edge cover, lap fabric 250mm.
5. Expansion joints to be 10mm thick, full depth closed cell cross linked polyethylene foam installed to manufacturers instructions. sealed with 10mm bead mastic/approved silicone.
6. Seek approval of location and levels prior to excavation.
7. Confirm service locations with relevant authorities on site prior to commencement.

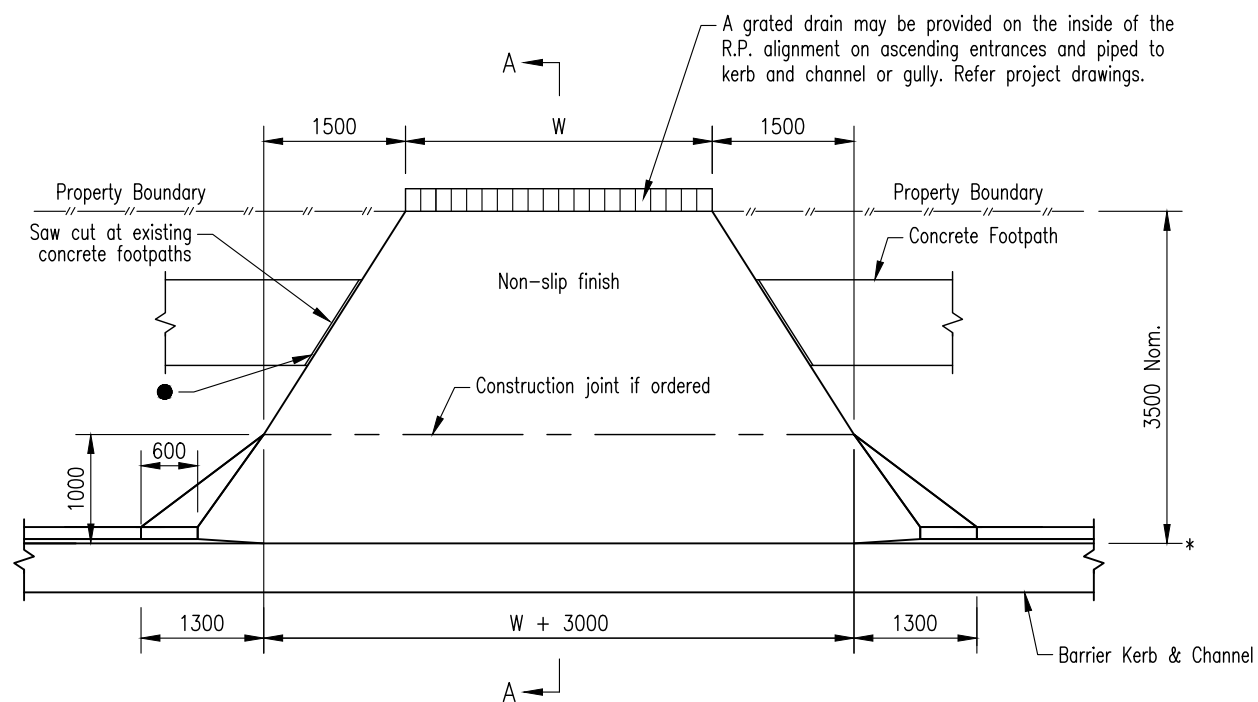


				Field Book No.	DRAWN L. Porter	<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>	Horiz. Section	STANDARD DRAWING – ROADS URBAN CONCRETE DRIVEWAYS	
				Level Book No.	DESIGNED S. Forbes		NOT TO SCALE		
D	Ableflex added at BOK	L.T.P.	09.02.15	Datum	CHECKED S. Forbes		Vert. Section NOT TO SCALE		
C	Barrier kerb dimensions	L.T.P.	30.08.13		EXAMINED G. Parsons				
B	Barrier kerb dimensions	L.T.P.	21.11.12		RECOMMENDED S. Hegedus RPEQ.				
A	Original Issue				TECHNICAL SERVICES MANAGER				
					DATE 13/07/2010				
Revisions		Drn by	Date	Job No./s	Works Order No.	Auxiliary Plan No's.	Plan No. R-005	No. 5 of 16 Plans	Rev. D

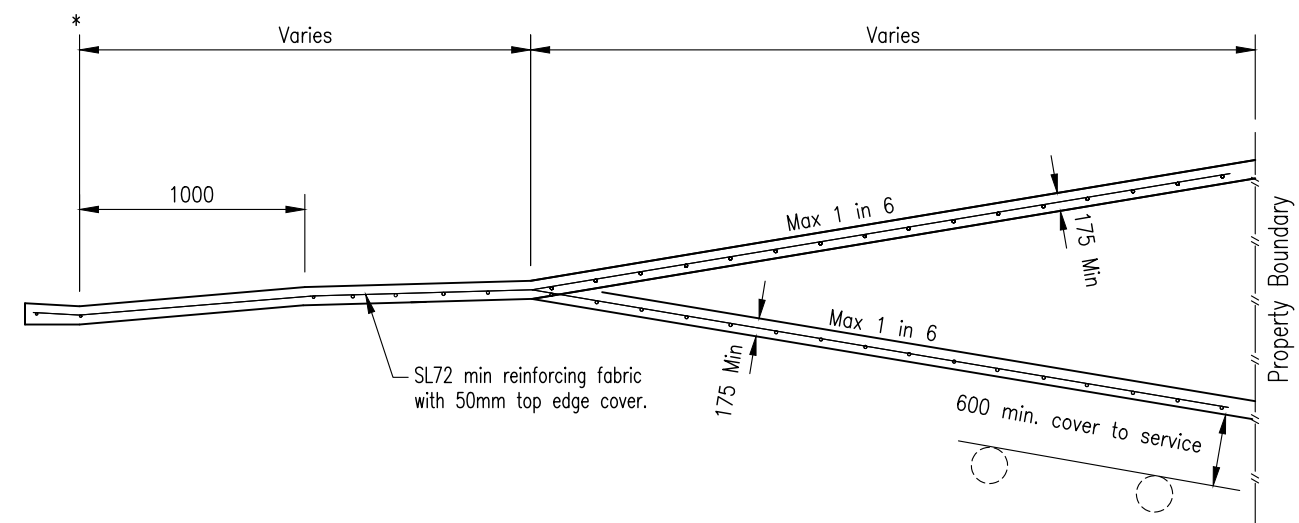




PLAN VIEW - WIDER FOOTPATHS



PLAN VIEW - 3.5m FOOTPATHS



SECTION A - A

#### LEGEND

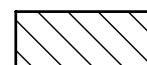
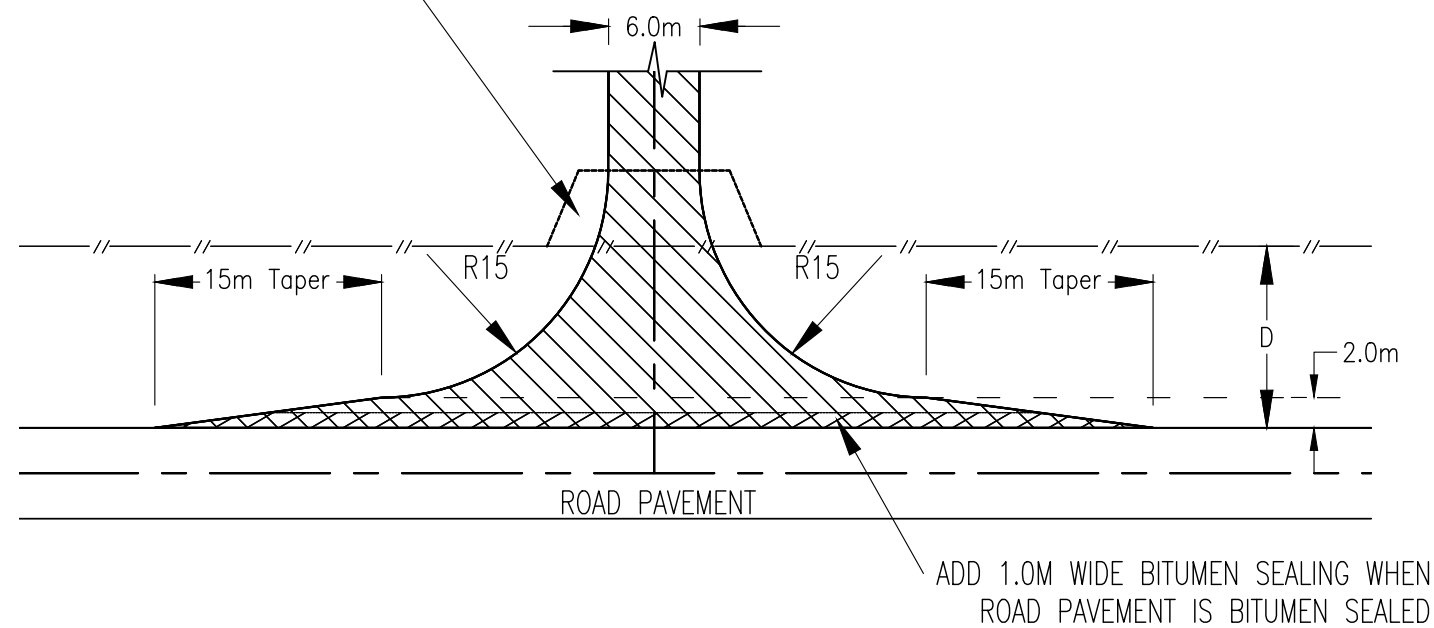
- \* Norm. Kerb Line
- Expansion joints to be 10mm thick, full depth closed cell cross linked polyethylene foam (85–150kg/m<sup>3</sup>) or 10mm thick compressed granulated corkboard. Installation to be as per manufacturers' instructions.

#### NOTES

1. Concrete N32 in accordance with Australian Standard 1379 and 3600.
2. Reinforcing fabric to Australian Standard 1304. Lap fabric 250mm.
3. Depths of concrete and reinforcing steel shown are the minimum requirements for good foundation conditions, and average traffic loading. Where this does not apply, depths of concrete and reinforcing shall be increased to suit specific conditions.
4. Design of crossings may vary, refer to project drawings.
5. Dimension W, 3.0m one way, 5.5m two way, refer specification or project drawings.
6. Reprofile adjacent footpath to match driveway. Footpath earthworks adjoining concrete must be well compacted.
7. Existing footpath profile to be maintained where possible.
8. Compaction for subgrade 95% standard to Australian Standard 1289.5.1.1.
9. Where subgrade is less than CBR 5 excavate and provide imported material to satisfaction of the Superintendent.
10. Driveway to be concrete unless otherwise approved.
11. Paver and/or proprietary concrete surface finishes to driveway shall only be used when approved by WDRC.
12. Refer project drawings or WDRC Standard Drawings for verge type cross sections.
13. All dimensions in millimetres unless otherwise stated.

				Field Book No.	DRAWN L. Porter		<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>	<div>Horiz. Section</div> <div>NOT TO SCALE</div> <div>Vert. Section</div> <div>NOT TO SCALE</div>	STANDARD DRAWING – ROADS COMMERCIAL DRIVEWAY SLAB		
				Level Book No.	DESIGNED S. Forbes						
					CHECKED S. Forbes						
				Datum	EXAMINED G. Parsons						
C	Legend symbol fixed	L.T.P.	24.10.16		RECOMMENDED S. Hegedus RPEQ.						
B	IPWEA Std Dwg note revised	L.T.P.	11.06.15		TECHNICAL SERVICES MANAGER						
A	Original Issue				DATE 14/07/2010						
Revisions		Drn by	Date		Job No./s	Works Order No.	Auxiliary Plan No's.		Plan No.R-006	No. 6 of 16Plans	Rev. C

FENCING TRUNCATION AT TANGENT OF ARCS WHERE 'D' IS LESS THAN 12m.



**Gravel:** Subbase Unbound Pavement to be specified by WDRC

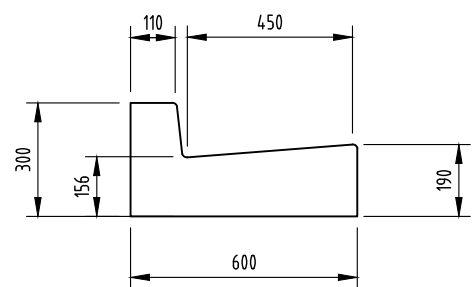
## TURNOUT DETAIL

Commercial and/or Truck

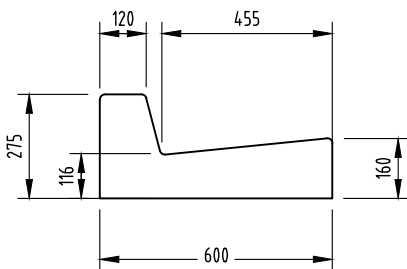
### NOTES

1. THE DIMENSIONS OF THE TURNOUT AND PAVEMENT DEPTHS SHALL NOT BE LESS THAN THOSE SHOWN ABOVE UNLESS OTHERWISE APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER.
2. QUALITY OF PAVEMENT MATERIAL FOR TURNOUT TO BE APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER.
3. INVERT LEVEL AT TURNOUT TO BE SUCH THAT FLOW IN TABLE DRAIN IS NOT RESTRICTED IN ANY WAY.
4. PERSONS CARRYING OUT WORK ON THEIR OWN BEHALF, WITH COUNCIL PERMISSION, SHALL BE RESPONSIBLE FOR THE SAFETY OF ROAD USERS AND WOULD BE WELL ADVISED TO HAVE PUBLIC RISK INSURANCE.
5. ACCEPTANCE OF PERMISSION FROM COUNCIL TO CONSTRUCT A PRIVATE TURNOUT SHALL INDEMNIFY THE COUNCIL FROM ANY CLAIM OR DEMAND WHATSOEVER.
6. ALL CULVERT OR PIPE WORK SHALL BE CARRIED OUT IN ACCORDANCE WITH Department of Transport and Main Roads STANDARD Drawings 1250 & 1260. IF PRECAST SLABS, ENDWALLS OR APRONS ARE APPROVED THEN ALL WORK SHALL BE CARRIED OUT TO THE MANUFACTURERS SPECIFICATIONS AND TO THE SATISFACTION OF COUNCIL'S TECHNICAL SERVICES MANAGER.
7. SIZE AND LOCATION OF UNDER DRAINAGE TO BE APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER.
8. SIGNAGE, LINEMARKING AND GUIDE POSTS (WHERE REQUIRED) ARE TO BE IN ACCORDANCE WITH QUEENSLAND TRANSPORT – DEPT OF MAIN ROADS, MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION).
9. FOR LIGHT TRAFFIC, REFER TO STANDARD DRAWING R-004: RURAL AND RESIDENTIAL TURNOUTS – INVERT AND CULVERT DRIVEWAYS.

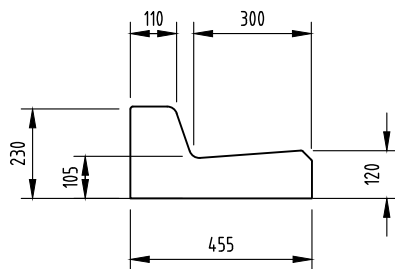
				Field Book No.	DRAWN P. Blissner	<div><div>WESTERN DOWNS</div><div>REGIONAL COUNCIL</div></div> <div><div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></div><div></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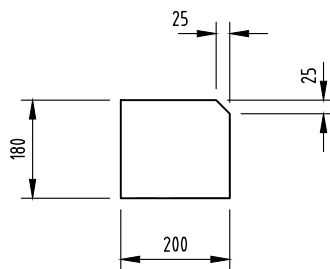
600mm BARRIER KERB 1  
(0.120m<sup>2</sup>)



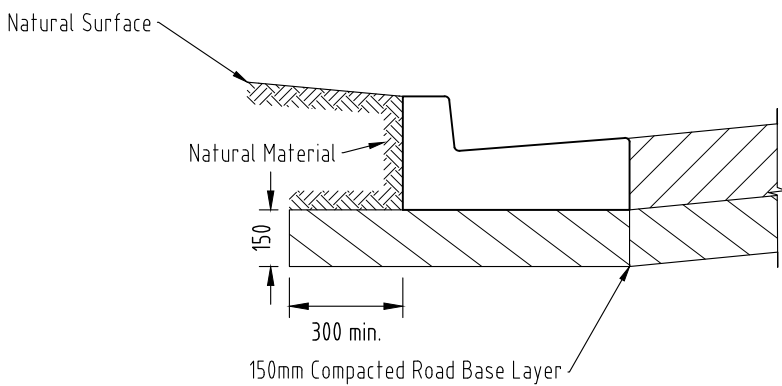
600mm WDRC BARRIER KERB 1  
(0.103m<sup>2</sup>)



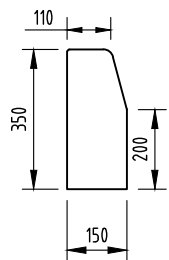
455mm BARRIER KERB 2  
(0.066m<sup>2</sup>)



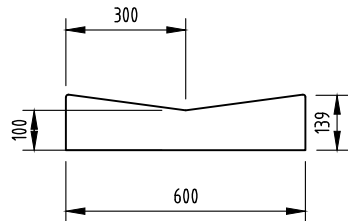
200mm EDGE BEAM  
(0.036m<sup>2</sup>)



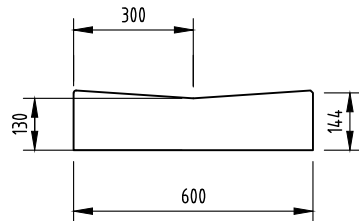
KERB AND ROAD BASE DETAIL



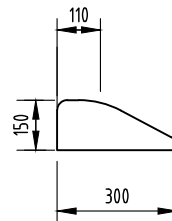
150mm STAND-UP KERB 1  
(0.049m<sup>2</sup>)



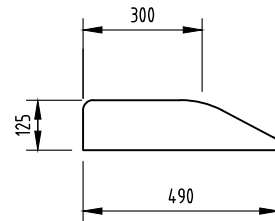
INVERT CHANNEL 1  
(0.072m<sup>2</sup>)



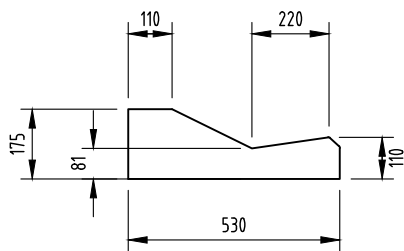
INVERT CHANNEL 2  
(0.084m<sup>2</sup>)



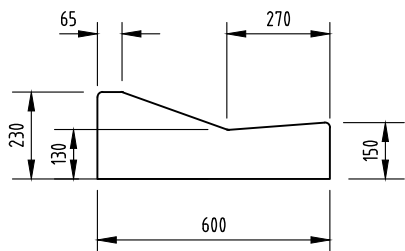
300mm ISLAND KERB 1  
(0.033m<sup>2</sup>)



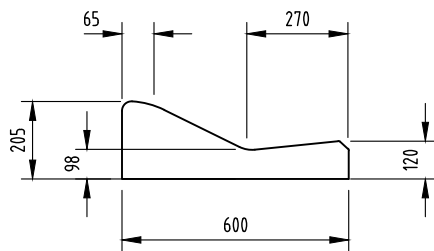
490mm ISLAND KERB 2  
(0.061m<sup>2</sup>)



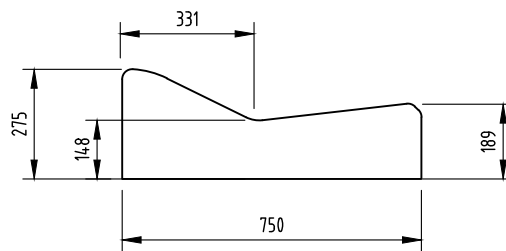
530mm MOUNTABLE KERB 1  
(0.066m<sup>2</sup>)



600mm WDRC MOUNTABLE KERB 2  
(0.103m<sup>2</sup>)



600mm MOUNTABLE KERB 2  
(0.074m<sup>2</sup>)




750mm MOUNTABLE KERB 3  
(0.144m<sup>2</sup>)

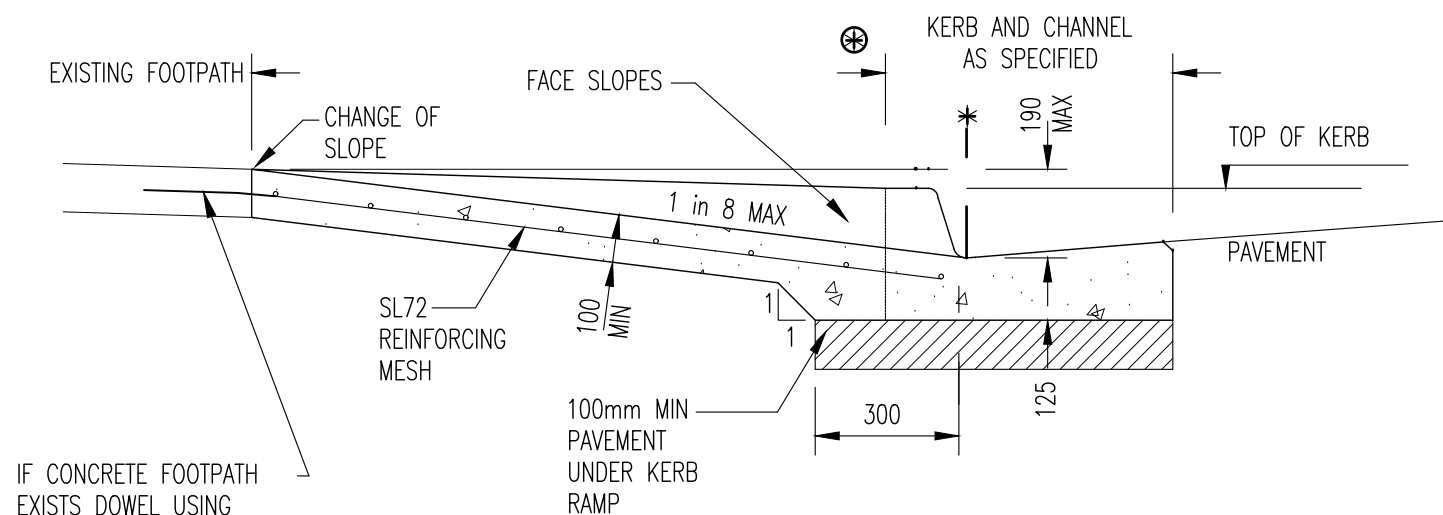
STANDARD KERB SECTIONS

NOTES

1. All materials and construction shall comply with AS 2876.
2. All concrete N32 minimum in accordance with AS 1379 and AS 3600 unless noted otherwise.
3. Control Joints shall be 3m centres unless noted otherwise.
4. Expansion joints at 12m centres unless noted otherwise.
5. Refer to Project Drawings for Kerb Setout.

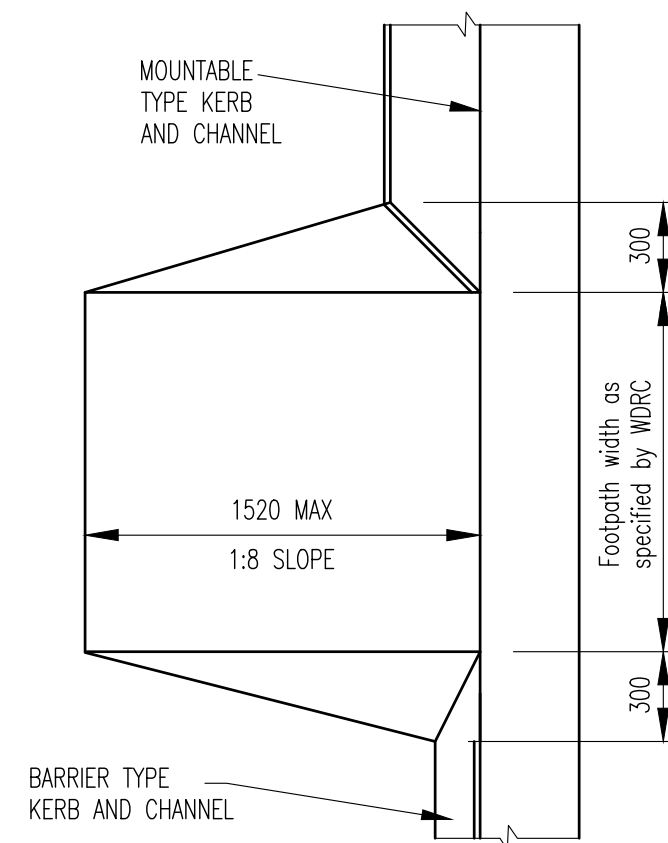
F	WDRC kerb and Mountable Added	D.R.H	12/12/17	Field Book No.	DRAWN L. Porter	<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>	Horiz. Section NOT TO SCALE	STANDARD DRAWING – ROADS KERBING PREPARATION DETAILS
E	Gravel Dimensions	D.R.H	04.04.17	Level Book No.	DESIGNED S. Forbes			
D	BK1 & MK2 dimensions	L.T.P.	30.08.13	Datum	CHECKED S. Forbes			
C	BK1 & MK2 dimensions	L.T.P.	21.11.12		EXAMINED G. Parsons			
B	Edge beam detail added	L.T.P.	14.02.12		RECOMMENDED S. Hegedus RPEQ.			
A	Original Issue				TECHNICAL SERVICES MANAGER		Vert. Section NOT TO SCALE	
Revisions		Drn by	Date		DATE 13/07/2010			
					Job No./s	Works Order No.	Auxiliary Plan No's.	



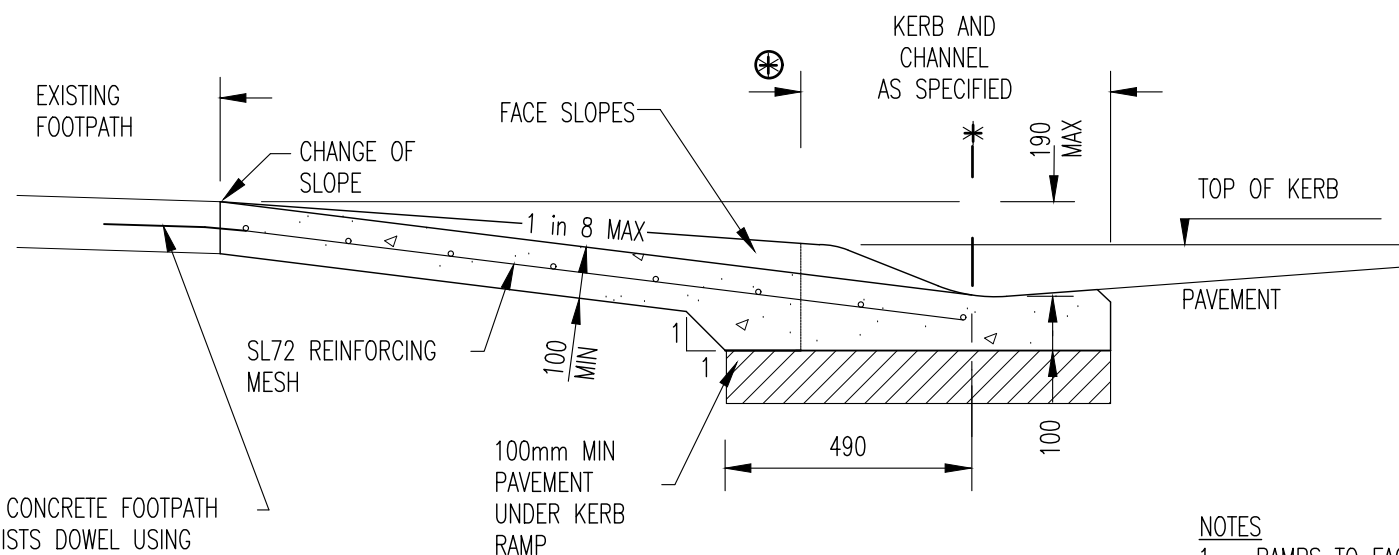


IF CONCRETE FOOTPATH EXISTS DOWEL USING Ø12mm HOT DIPPED GALVANISED DOWEL BARS 450MM LONG @ 300mm CRS.

**SECTION AT BARRIER KERB & CHANNEL**



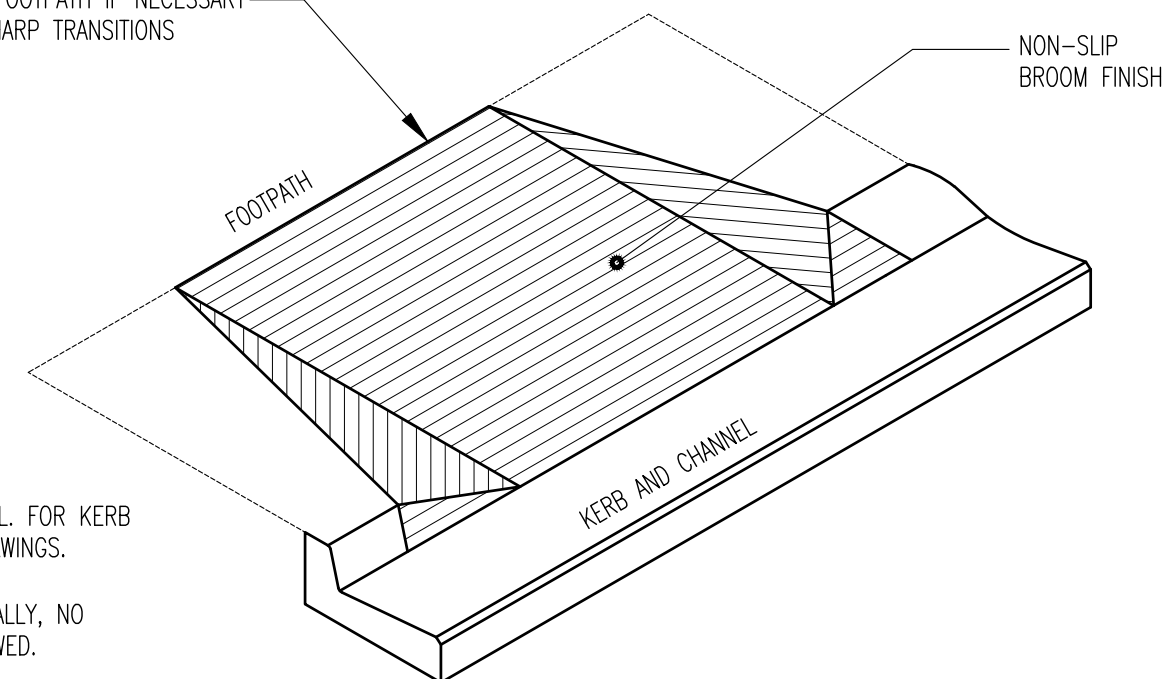
**KERB RAMP**



IF CONCRETE FOOTPATH EXISTS DOWEL USING Ø12mm HOT DIPPED GALVANISED DOWEL BARS 450mm LONG @ 300mm CRS.

**SECTION AT MOUNTABLE KERB & CHANNEL**

RECONSTRUCT FOOTPATH IF NECESSARY TO PROVIDE SHARP TRANSITIONS

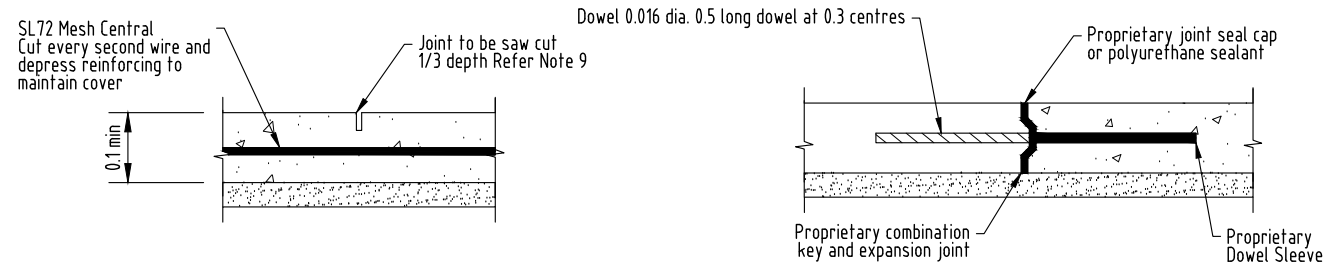
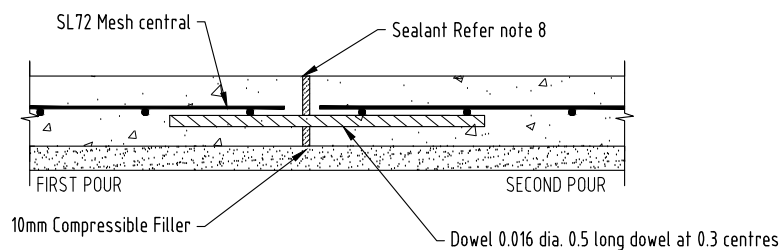
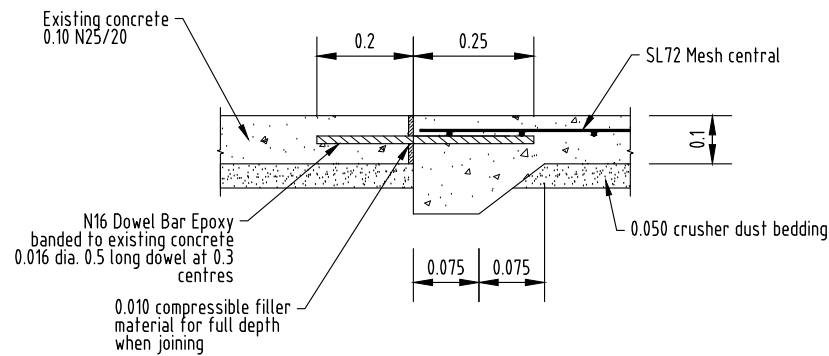
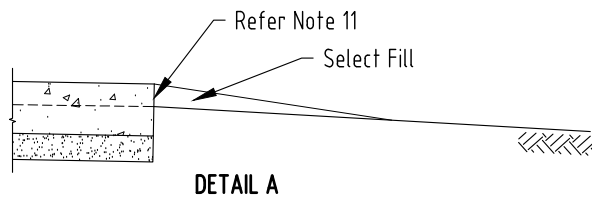
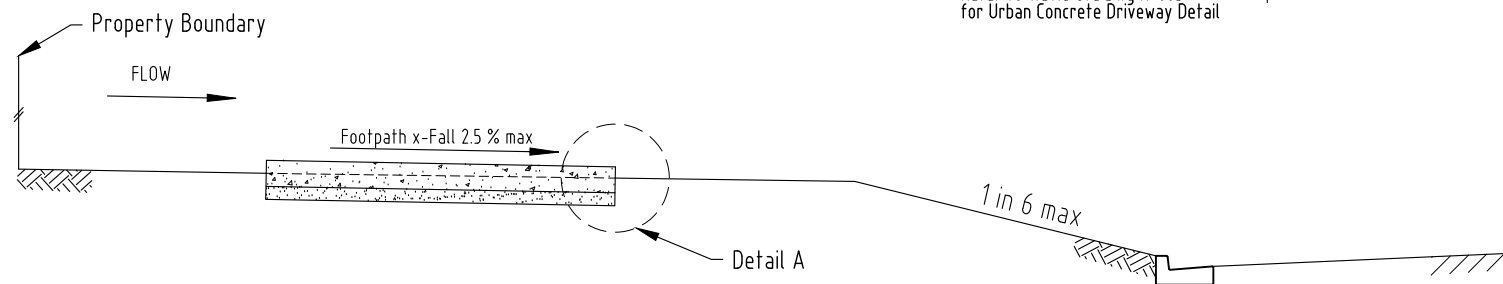
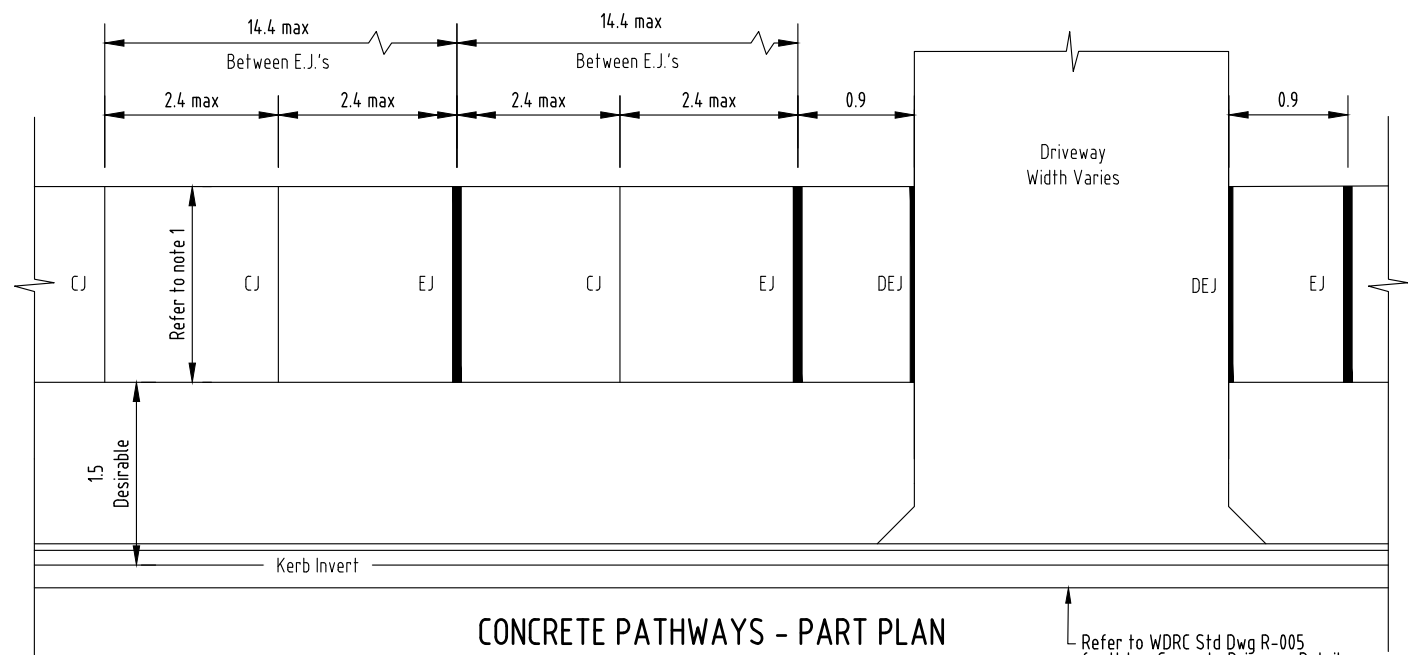


**PERSPECTIVE VIEW**

**NOTES**

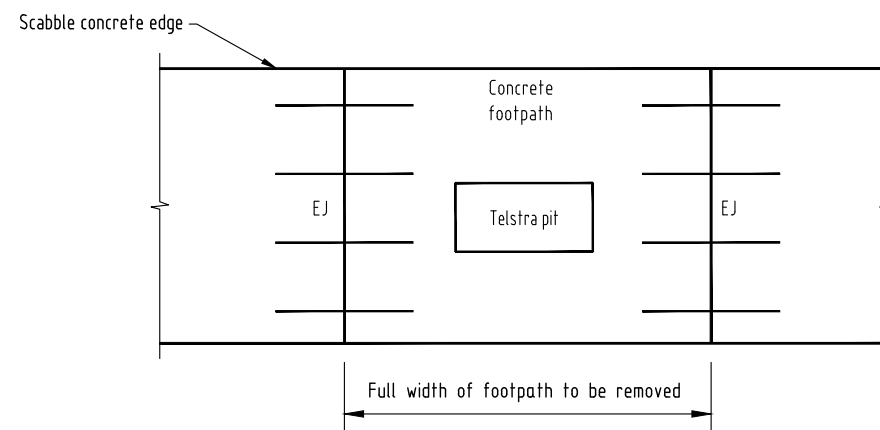
1. RAMPS TO FACE DIRECTION OF TRAVEL. FOR KERB RAMP LOCATION, REFER PROJECT DRAWINGS.
2. CONCRETE N32/20.
3. KERB RAMP TO BE CAST MONOLITHICALLY, NO CONSTRUCTION JOINTS WILL BE ALLOWED.
4. ALL DIMENSIONS IN MILLIMETRES.
5. FOOTPATH WIDTH TO BE SPECIFIED BY WDRC.

Revisions		Drn by	Date	Field Book No.	Level Book No.	DRAWN L. Porter		<div>WESTERN DOWNS REGIONAL COUNCIL</div> 	Horiz. Section	STANDARD DRAWING – ROADS KERB RAMP
						DESIGNED S. Forbes	NOT TO SCALE			
						CHECKED S. Forbes				
						EXAMINED G. Parsons				
						RECOMMENDED S. Hegedus RPEQ. 5234				
						TECHNICAL SERVICES MANAGER				
						DATE 14/07/2010				
						Job No./s	Works Order No.	Auxiliary Plan No's.		
B	Footpath Width Changed	S.E.R.	25.05.18							
A	Original Issue								Plan No. R-009 No. 9 of 16 Plans Rev. B	

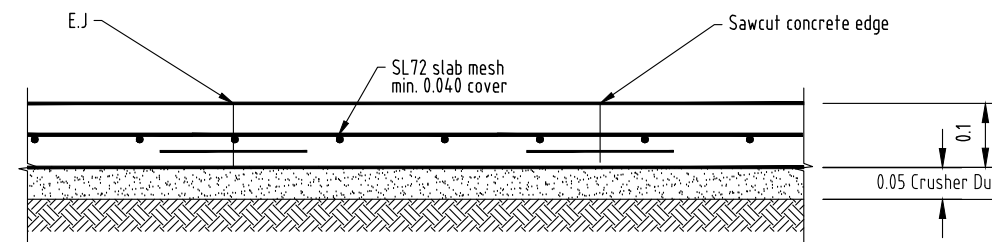


CONTROL JOINT (CJ)

EXPANSION JOINT (EJ)



TELSTRA PIT WORKS PLAN VIEW



TELSTRA PIT WORKS SECTION VIEW

PATH WIDTHS

Footpath	1.5m
Bikeway	2.0m
Shared Footpath	2.5m

NOTES

- Refer to table on this plan for path widths.
- Concrete to be grade N32 AS 1379 and AS 3600 unless approved otherwise
- Reinforcing requirements may be amended on written instruction from Site Engineer
- For Steel mesh reinforced paths at dowelled expansion joints: Mesh to be stopped 0.075 from the joint, be placed under dowels and chairs at min 0.05 cover from bottom to deter the mesh from deflection interfering with the dowels.
- Surface to be non-slip concrete finish to AS 1428.1
- Where a vehicle crossing point, or path is subject to longitudinal traffic the pathway details shall be per relevant residential driveway standard refer to WDRC Std Dwg. R-005 and R-006.
- Additional path details shall be as per Austroads Guidelines.
- Expansion joints to be sealed with a low modulus self priming sealant to the manufacturers specifications. The colour of the sealant is to match the adjoining surface finish.
- Saw cut joints are to be undertaken between 4 to 12 hours after laying depending on conditions. Joint sealant is required in sandy areas.
- All measurements are in metres.
- Concrete Footpath should Nominally to be placed 0.020 -0.050 above average natural surface (Contractor should check with Project Manager prior to construction).

G	Overall Review	D.R.H	13.12.17
F	Review	D.R.H	04.04.17
E	Notes & drafting changes	L.T.P.	24.04.15
D	Drafting changes	L.T.P.	17.03.15
C	E.J. Fibreboard change	L.T.P.	24.09.13
B	Shared footpath added	S.E.R.	16.05.13
A	Original Issue	L.T.P	15.07.10
Revisions		Drn by	Date

Field Book No.	
Level Book No.	
Datum	

DRAWN	D.Hubner
DESIGNED	R.Hazzard
CHECKED	P.Blisner
EXAMINED	R.Hazzard
RECOMMENDED	R.Hazzard RPEQ.17538
DATE 13/12/2017	
Job No./s	Works Order No.



Horiz. Section  
NOT TO SCALE  
Vert. Section  
NOT TO SCALE

**STANDARD DRAWING – ROADS**  
**TYPE CROSS SECTION FOR CONCRETE**  
**FOOTPATH TYPES WITH JOINING DETAILS**  
Plan No.**R-010** No. 10 of 16Plans Rev. **F**

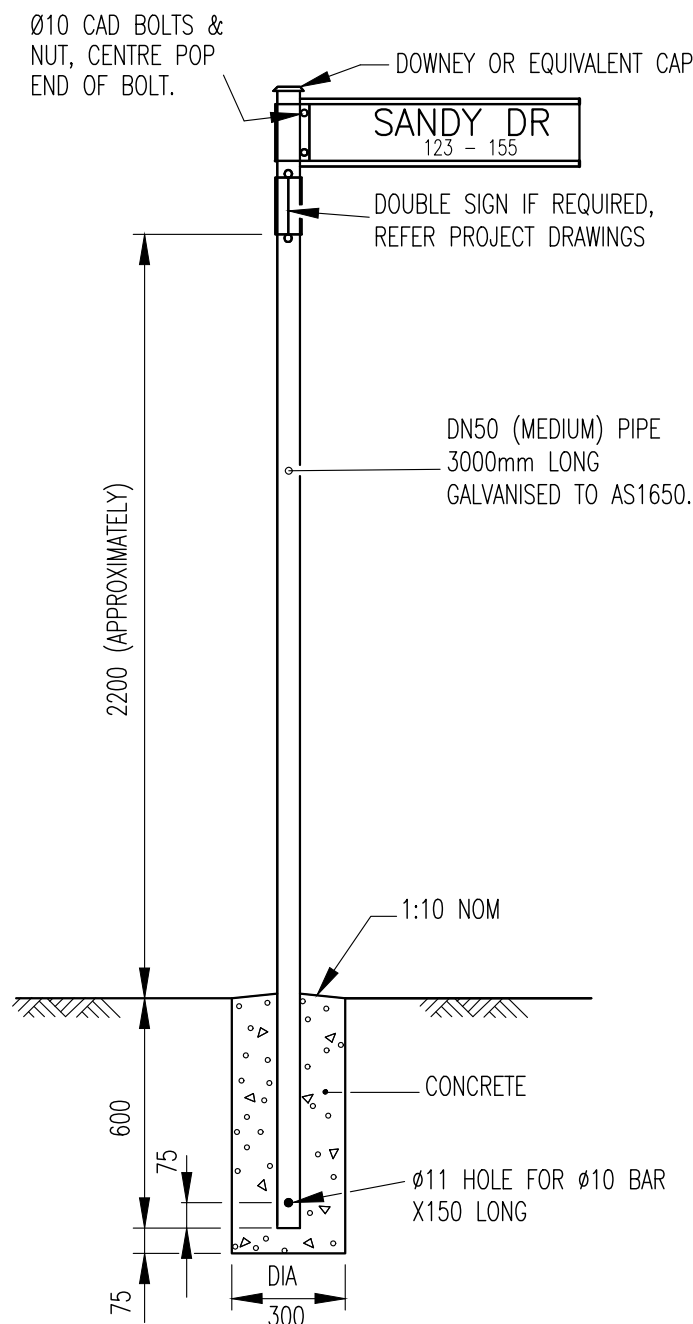
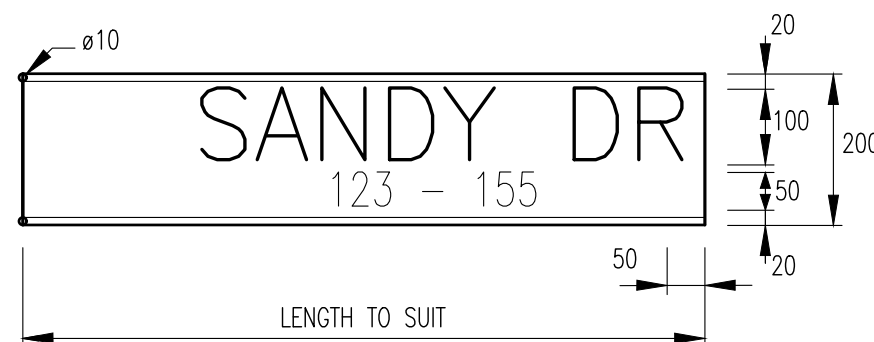


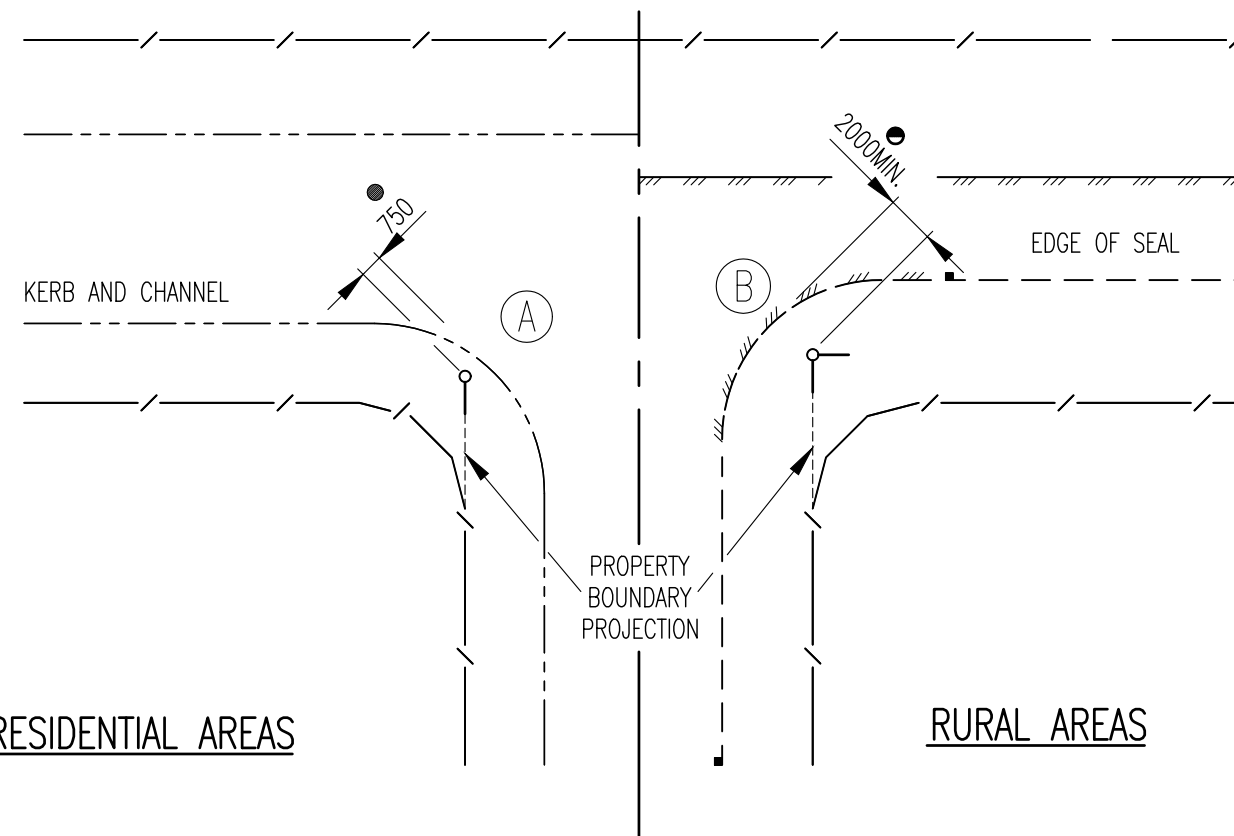
TABLE OF ABBREVIATIONS	
AVENUE	AV
COURT	CT
CRESCENT	CR
DRIVE	DR
ESPLANADE	ESP
LANE	LA
PARADE	PDE
ROAD	RD
STREET	ST
TERRACE	TCE

OTHER ABBREVIATIONS TO BE APPROVED BY SUPERINTENDENT



#### LEGEND

- SIGN POST IS TO BE LOCATED 750mm BEHIND NORMAL KERB LINE.
- SIGN POST IS TO BE LOCATED 2000mm MIN. - 4000mm MAX. FROM EDGE OF SEAL, OR AS DIRECTED BY THE SUPERINTENDENT.



RESIDENTIAL AREAS

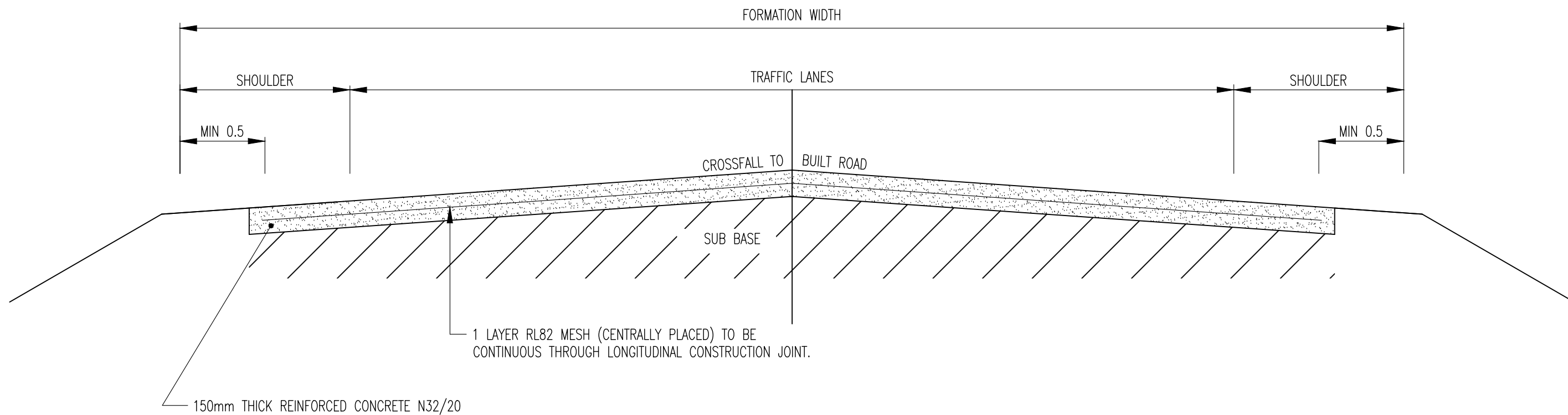
RURAL AREAS

#### SIGN LOCATIONS

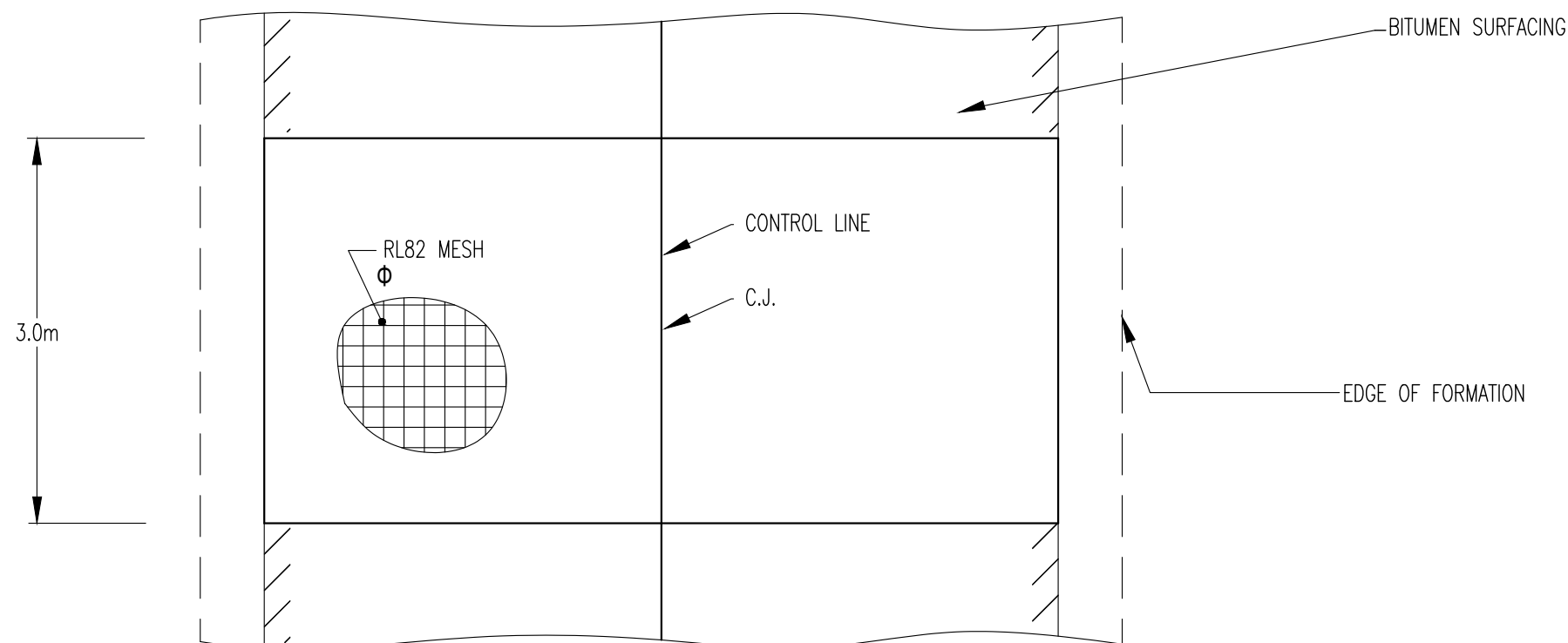
#### NOTES

- STREET NAMES MUST BE APPROVED BY COUNCIL.
- NAME PLATES: 200mm WIDE AND 3mm THICK EXTRUDED ALUMINUM OR POLYPROPYLENE SECTION.
- BRACKET: STANDARD 200mm WIDE AND 3mm THICK ALUMINUM EXTRUDED BRACKET (INCLUDING 2 X Ø6 CAD BOLTS & NUTS). CAD BOLTS AND NUTS TO AS 1897.
- LETTERS & NUMBERS : CLASS 1 BLACK ON CLASS 2 WHITE REFLECTORISED BACKGROUND (BOTH SIDES) TO AS 1743.2  
LETTERS: 100mm HIGH, SERIES B, MEDIUM SPACING.  
(SPACING MAY BE VARIED TO SUIT LENGTH OF STREET NAME WHEN APPROVED BY SUPERINTENDENT.)  
NUMBERS: 50mm HIGH, SERIES C, NARROW SPACING.  
ALL TEXT TO AS1744.
- ALL SIGNS ARE TO BE APPROVED BY THE SUPERINTENDENT PRIOR TO ERECTION.
- SIGNS TO BE POSITIONED ON THE SIDE OF STREET/ROAD THAT PROVIDES BEST VISIBILITY.
- CONCRETE N20 IN ACCORDANCE WITH AS 1379 AND AS 3600.
- BARS Ø10, GRADE 250 TO AS 1302.
- ALL DIMENSIONS IN MILLIMETRES.

				Field Book No.	DRAWN L. Porter	<b>WESTERN DOWNS</b> REGIONAL COUNCIL 	Horiz. Section	STANDARD DRAWING – ROADS
				Level Book No.	DESIGNED S. Forbes		NOT TO SCALE	
				Datum	CHECKED S. Forbes		Vert. Section	
D	Road number removed	L.T.P.	05.03.15		EXAMINED G. Parsons		NOT TO SCALE	STREET SIGNS AND LOCATION OF
C	Council logo note removed	L.T.P.	24.07.14		RECOMMENDED S. Hegedus RPEQ.			STREET SIGNS
B	Logo replaced by road no.	L.T.P.	07.03.13		TECHNICAL SERVICES MANAGER			
A	Original Issue				DATE 14/07/2010			
Revisions		Drn by	Date		Job No./s	Works Order No.	Auxiliary Plan No's.	
							Plan No. R-011 No. 11 of 16 Plans Rev. D	



**TYPE CROSS SECTION**



**PLAN VIEW**

Φ REINFORCING MESH SHALL END 75mm FROM THE ENDS AND 50mm FROM THE SIDES.

				Field Book No.	DRAWN L. Porter	<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>		Horiz. Section	STANDARD DRAWING – ROADS CONCRETE MACHINERY CROSSING		
				Level Book No.	DESIGNED S. Forbes			NOT TO SCALE			
				Datum	CHECKED S. Forbes			Vert. Section			
					EXAMINED G. Parsons						NOT TO SCALE
					RECOMMENDED S. Hegedus RPEQ. TECHNICAL SERVICES MANAGER						
A	Original Issue				DATE 14/07/2010						
Revisions		Drn by	Date		Job No./s	Works Order No.	Auxiliary Plan No's.		Plan No.R-012	No. 12 of 16Plans	Rev.A

NOTES

1. G.S.J. AND E.J. REFER TO STD DWG R-009 FOR CONSTRUCTION DETAILS.  
2. ALL CONCRETE WORK SHALL BE IN ACCORDANCE WITH AS 3600  
3. CONCRETE SPECIFICATION SHALL BE:

ELEMENT	GRADE	SLUMP	MAX AGG.
FILLING 200 CMB	N20	230 +/-30	10
GENERAL	N25	80 +/-15	20
STRUCTURAL	REFER TO NOTES ON DRAWINGS		

4. COVER TO REINFORCEMENT SHALL BE:  
a. FOOTING 65mm BTM, 50mm TOP AND SIDES.  
b. GROUND FLOOR SLABS 30mm TOP.  
c. EXTERNAL SUSPENDED SLABS 40mm TOP AND BOTTOM.  
d. INTERNAL SUSPENDED SLABS 20mm TOP AND BOTTOM.  
e. BEAMS 40mm  
f. COLUMNS 40mm  
g. WITHIN CONCRETE MASONRY BLOCK 10mm  
5. REINFORCEMENT SHALL BE SUPPORTED ON APPROVED PLASTIC OR PLASTIC TIPPED WIRE CHAIRS AS FOLLOWS:  
a. 'N' BARS UP TO N12 AND FABRIC 800mm.  
b. 'N' BARS N16 AND LARGER 1200mm.

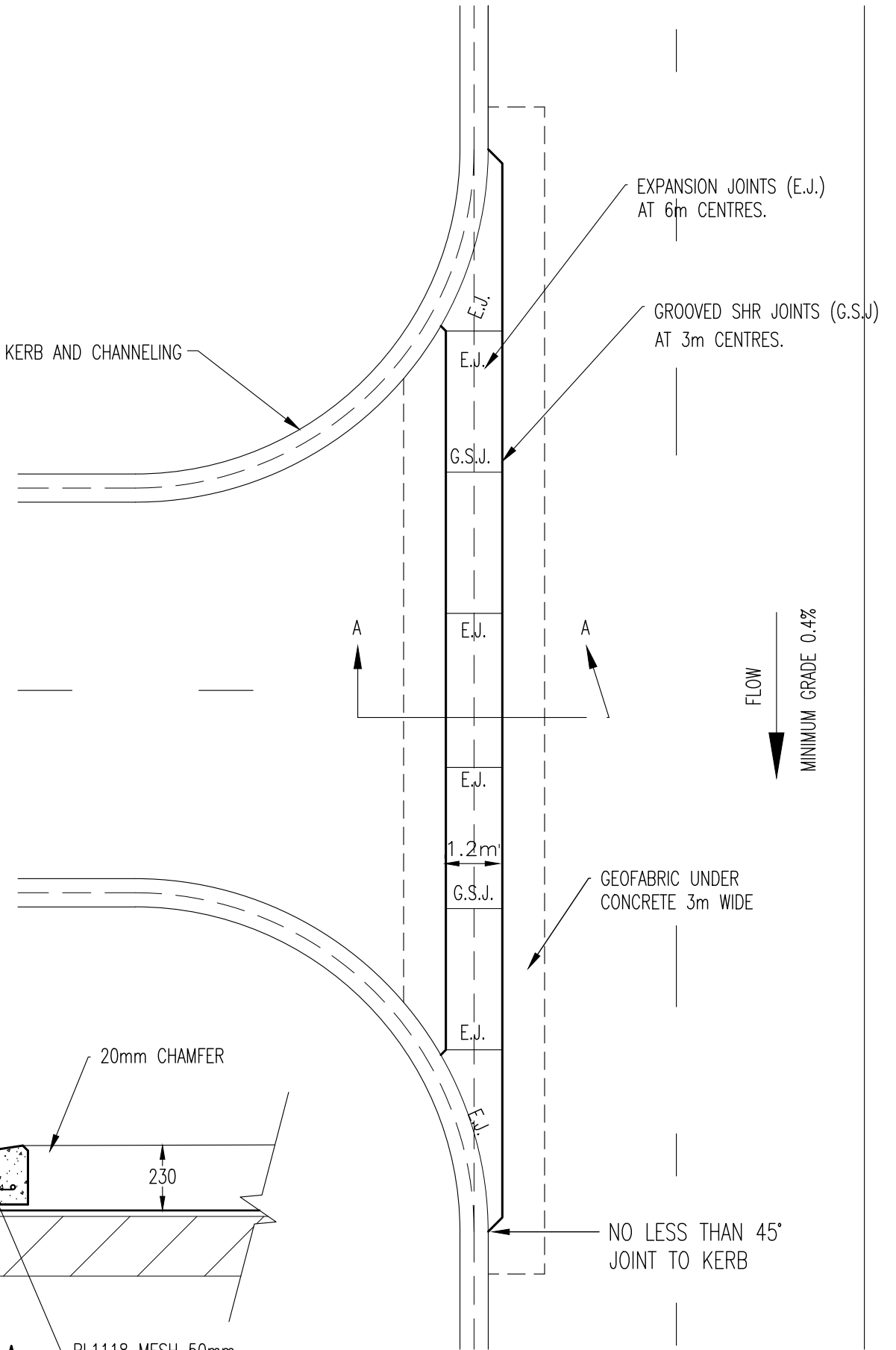
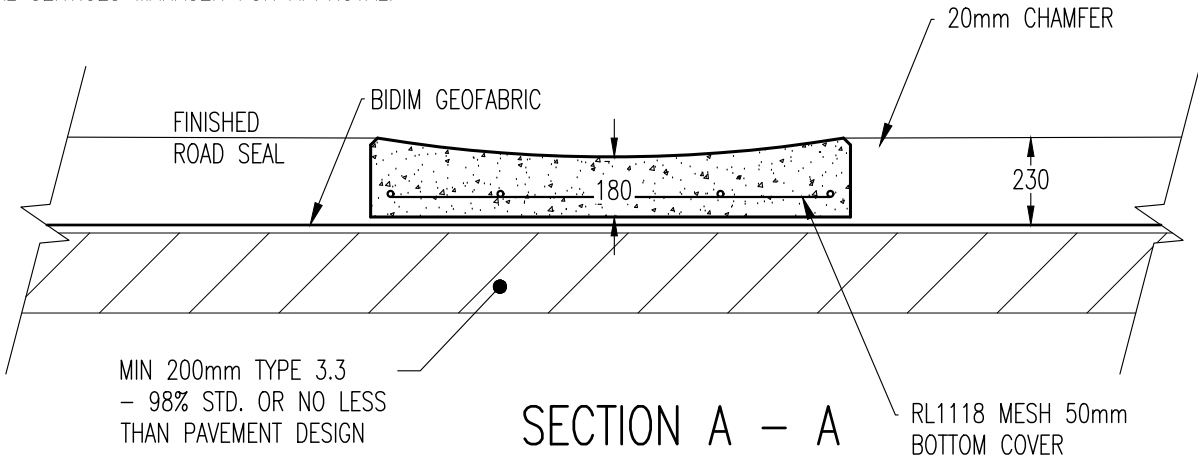
WELDING OF REINFORCEMENT IS NOT PERMITTED UNLESS SHOWN ON THE DRAWINGS OR APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER.


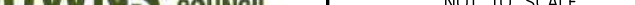
6. LAPS UNLESS OTHERWISE NOTED. (UNO)  
FABRIC 1 COMPLETE MESH.

N12-600mm  
N20-1000mm  
N28-1400mm  
N16-800mm  
N24-1200mm

7. CONSTRUCTION JOINTS SHALL BE SCABBLED AND CLEANED AND COATED WITH A CEMENT/WATER SLURRY IMMEDIATELY PRIOR TO PLACING CONCRETE.  
8. CONCRETE SHALL BE COMPACTED USING MECHANICAL VIBRATORS.  
9. CONCRETE SHALL BE CURED FOR A MINIMUM OF 7 DAYS BY A METHOD APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER.  
10. NO HOLES OR CHASES OTHER THAN THOSE SHOWN ON THE STRUCTURAL DRAWINGS SHALL BE MADE IN CONCRETE MEMBERS UNLESS APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER.  
11. FORMWORK SHALL COMPLY WITH AS 3610 FORMWORK WORK CODE AND ALL RELEVANT CONSTRUCTION SAFETY LEGISLATION. FINISHES SHALL BE AS SPECIFIED IN THE ARCHITECTURAL DRAWINGS AND SPECIFICATIONS. MINIMUM STRIPPING TIMES (UNLESS OTHERWISE APPROVED BY COUNCIL'S TECHNICAL SERVICES MANAGER) SHALL BE IN ACCORDANCE WITH AS 3610 FORMWORK CODE.  
12. CONTROL JOINTS SHALL BE CONSTRUCTED AS SPECIFIED. SAW CUTTING SHALL BE CARRIED OUT WITHIN 8 HOURS OF CONCRETE HARDENING.  
13. CONCRETE TEST CYLINDERS SHALL BE TESTED IN ACCORDANCE WITH AS 3600-CLAUSE 20.3 BY AN N.A.T.A. REGISTERED LABORATORY. ALL RESULTS ARE TO BE FORWARDED TO COUNCIL'S TECHNICAL SERVICES MANAGER FOR APPROVAL.  
14. PROJECT CONTROL TESTING IN ACCORDANCE WITH THE FOLLOWING TABLE:

NUMBER OF BATCHES PER DAY.	NUMBER OF SAMPLES.
1.	1.
2 TO 5.	2.
6 TO 10.	3.
11 TO 20.	4.
FOR EACH ADDITIONAL 10 BATCHES, 1 ADDITIONAL SAMPLE SHALL BE TAKEN.	

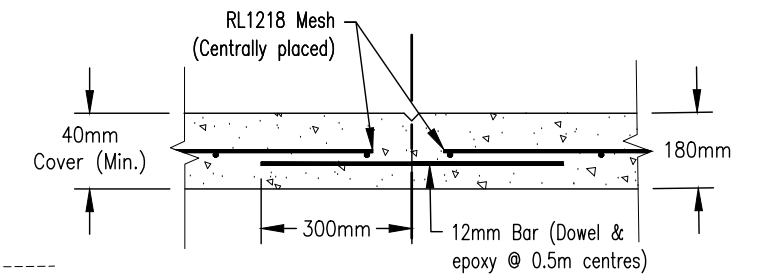
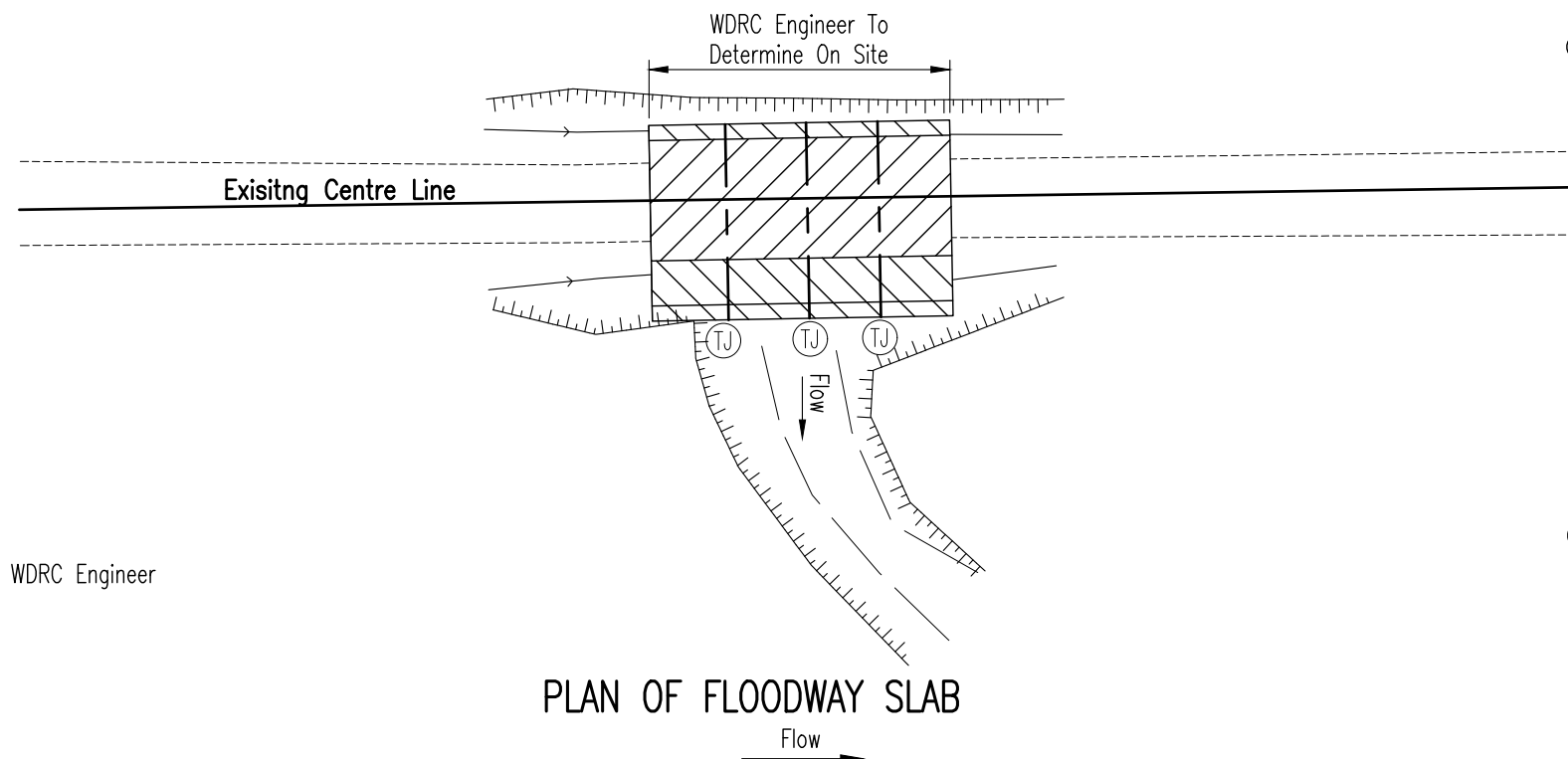


				Field Book No.	DRAWN S. Robertson			Horiz. Section NOT TO SCALE	STANDARD DRAWING – ROADS INVERT DRAIN AT ROAD INTERSECTION
				Level Book No.	DESIGNED S. Forbes				
				Datum	CHECKED S. Forbes				
					EXAMINED G. Parsons				
					RECOMMENDED S. Hegedus RPEQ.				
C	Conc. Depth Changes	S.E.R	12.10.15	Revisions	TECHNICAL SERVICES MANAGER			Vert. Section NOT TO SCALE	Plan No. R-013 No. 13 of 16 Plans Rev. C
B	RL1118 Mesh note edited	L.T.P.	17.02.15		DATE 08/01/2011				
A	Original Issue				Job No./s	Works Order No.			
Revisions			Drn by	Date					

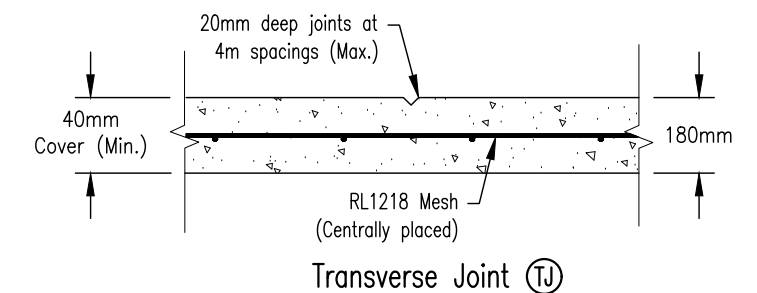




Refer to MUTCD for signage requirements if required.



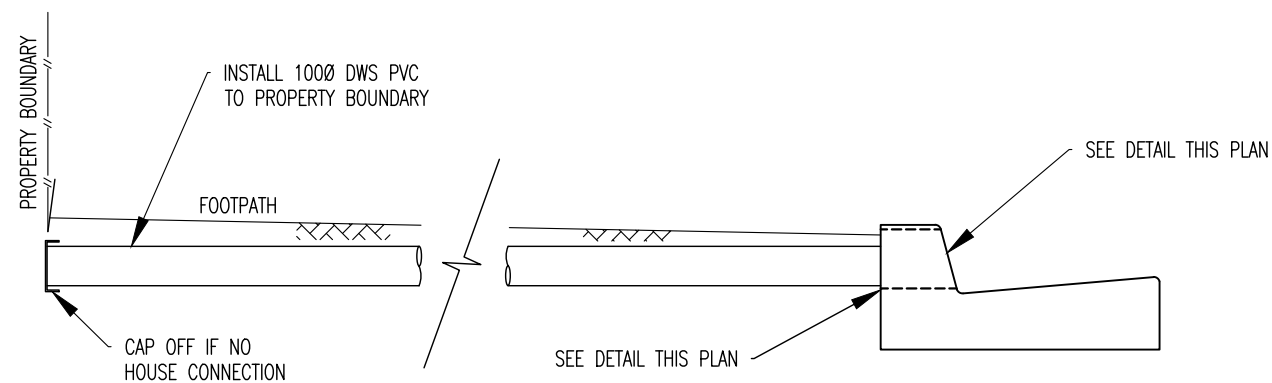
NOTE: Central construction joint can be transverse joint if ordered by Site Engineer. This allows monolithic concrete pouring across traffic lanes.



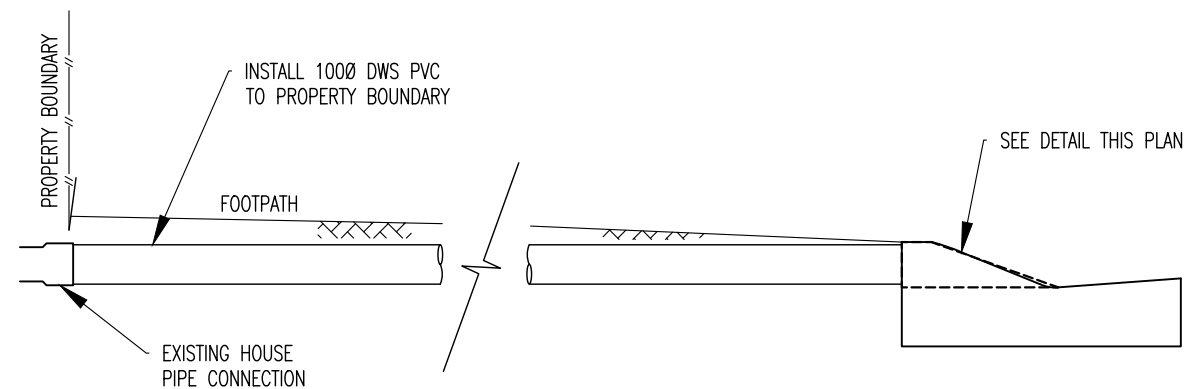
Notes:

1. Selected rock and gravel fill as required to bring to subgrade level.
2. Add Base Gravel to give a minimum of 200mm compacted layer below concrete.
3. Reinforced concrete pavement (N32/20) @ 180mm thick, w/- 1 layer of RL1218 mesh centrally placed for main slab. Mesh overlap 200mm min.
4. Protection Works:  
Batters, Aprons and Curtain walls at 150mm thick, w/- SL62 mesh centrally placed as per drawing and 12mm bar (600mm long) seated to 300mm deep @ 500mm centres and epoxyed into main slab.
5. Provide 75mm  $\varnothing$  Pressure Relief Holes at 2.0m centres horizontally (D/S side only). No fines concrete blocks behind each weephole.

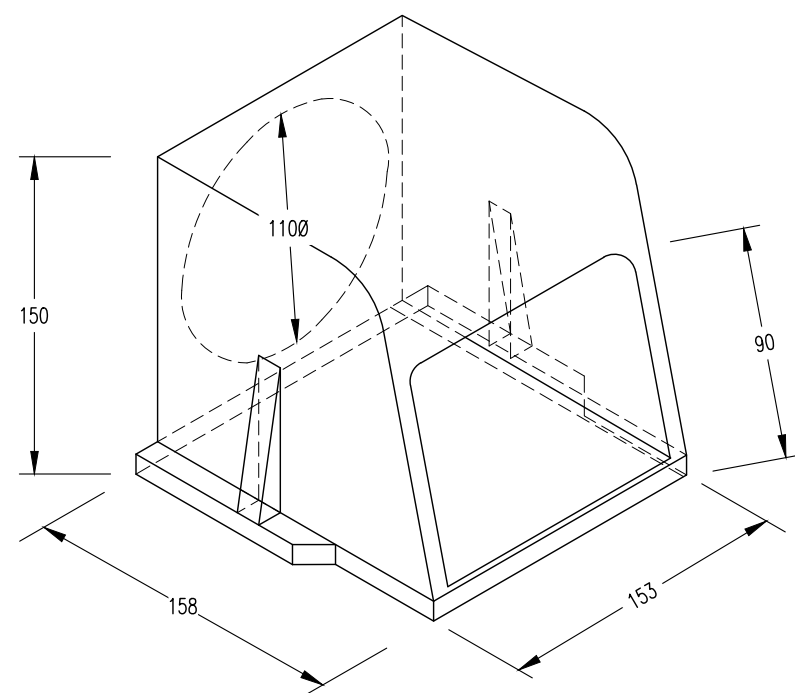
				Field Book No.	DRAWN N. Palm		<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>	Horiz. Section	<div>STANDARD DRAWING – ROADS</div> <div>TYPICAL CONCRETE FLOODWAY</div>		
E	Remove signs,note changes	PEB	30.01.17	Level Book No.	DESIGNED S. Forbes			NOT TO SCALE			
D	Joints changes	S.J.F.	27.01.15	Datum	CHECKED S. Forbes						
C	Mesh type and label changes	L.T.P.	18.06.14		EXAMINED G. Parsons						
B	Various label changes	L.T.P.	28.02.12		RECOMMENDED RPEQ.						
A	Original Issue				TECHNICAL SERVICES MANAGER						
					DATE 01/03/2011						
Revisions		Drn by	Date		Job No./s	Works Order No.	Auxiliary Plan No's.		Plan No. <i>R-015</i>	No. 15 of 16Plans	Rev. <i>E</i>



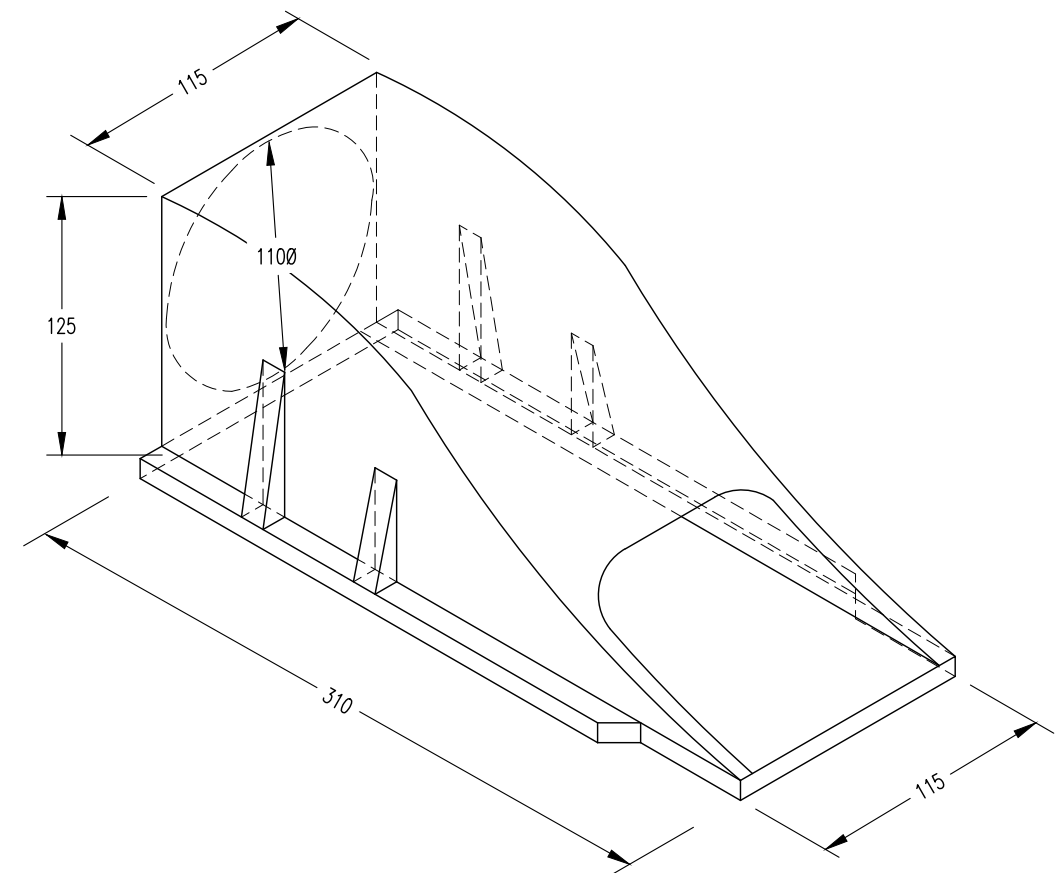
**BARRIER KERB AND CHANNEL SECTION**



**MOUNTABLE KERB AND CHANNEL SECTION**



**BARRIER KERB ADAPTOR**

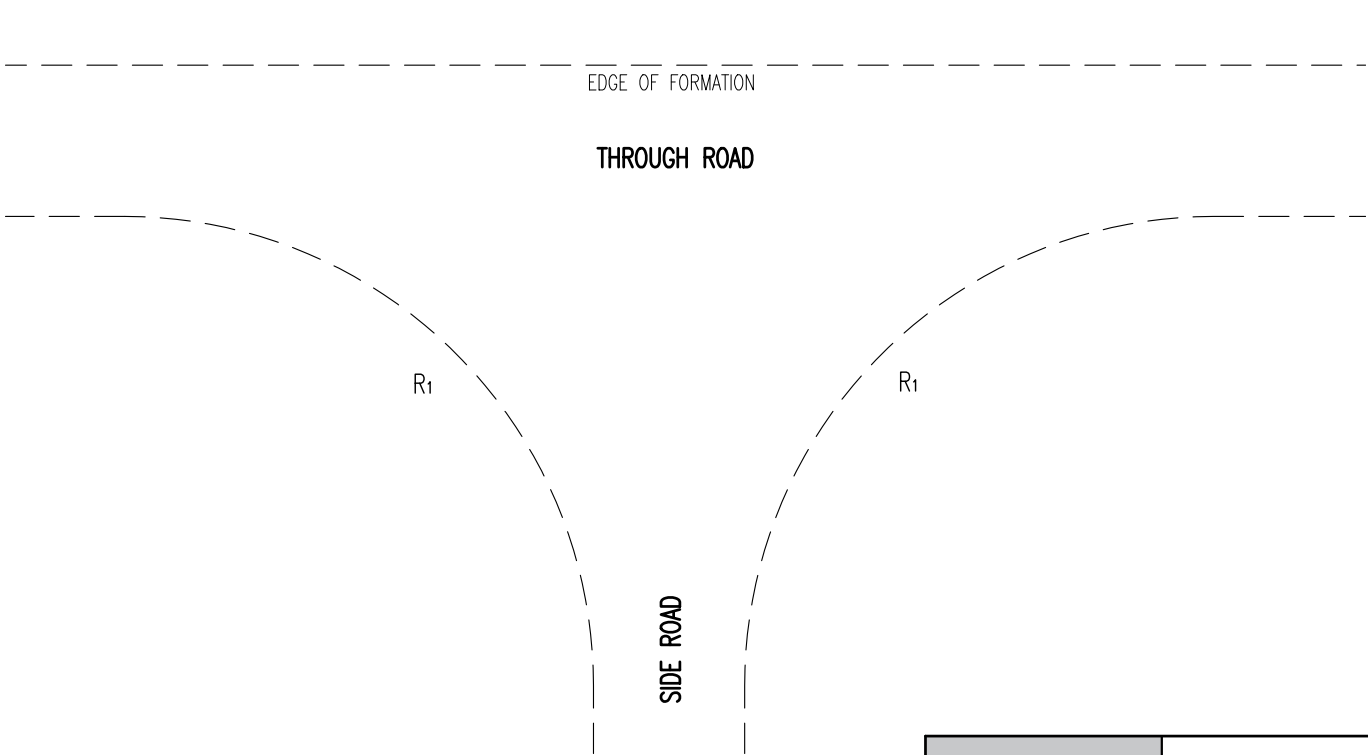


**MOUNTABLE KERB ADAPTOR**

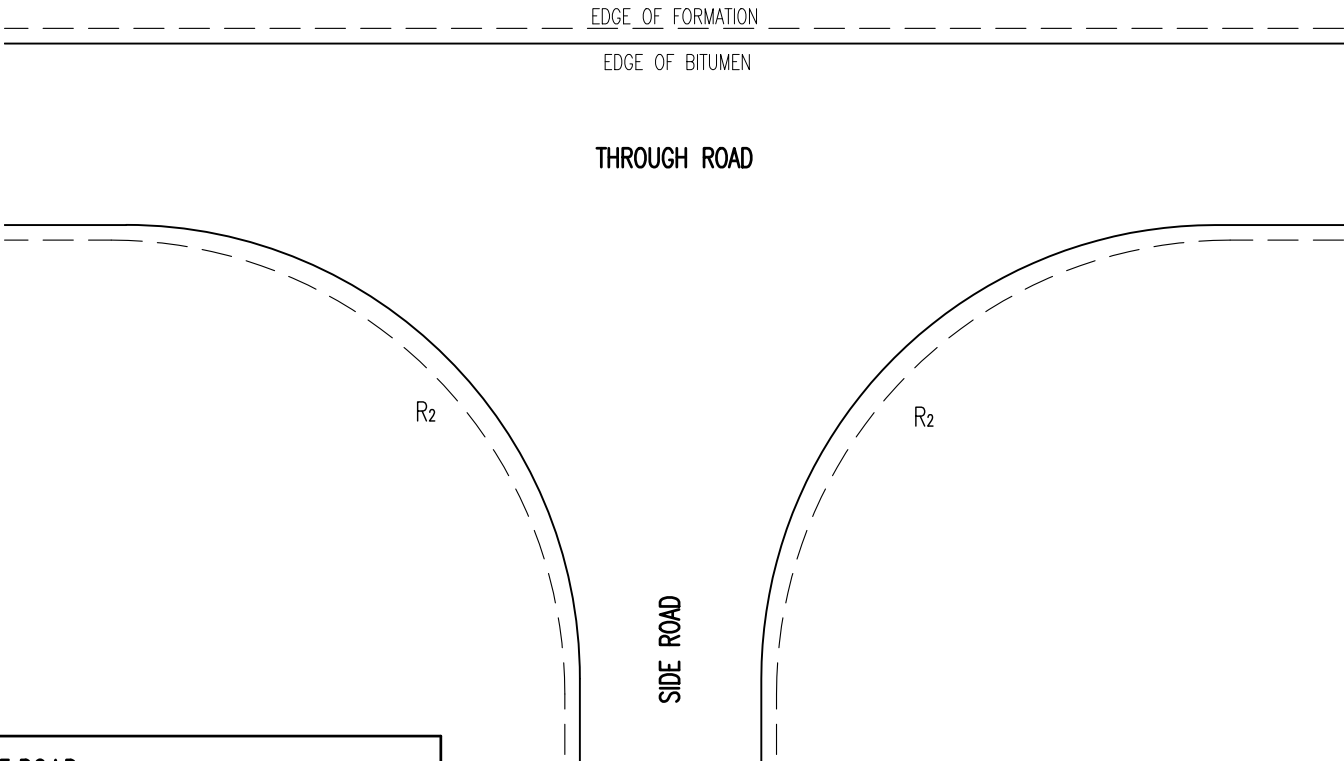
**NOTES**

1. CAST IRON MOUNTABLE AND BARRIER KERB ADAPTOR TO BE MANUFACTURED BY WHITE INDUSTRIES, DALBY. WHITE INDUSTRIES CAN BE CONTACTED ON (07) 4672 5800.
2. STANDARD 1000 DWS PVC ROOFWATER PIPE TO BE LAID WITH MAXIMUM AVAILABLE COVER. PIPE MUST HAVE MINIMUM GRADE OF 1 IN 80. LAY BACK TO PROPERTY BOUNDARY AND MAKE CONNECTION TO EXISTING HOUSE SERVICE OR CAP OFF FOR FUTURE CONNECTION.
3. AT EXISTING KERB AND CHANNEL SAW CUT KERB AS NECESSARY. REINSTATE WITH N20/10 CONCRETE AROUND KERB ADAPTOR.
4. ALL DIMENSIONS IN MILLIMETRES.
5. REFER TO WESTERN DOWNS REGIONAL COUNCIL STANDARD DRAWING R-008 FOR KERB PREPARATION DETAILS.
6. SAW CUT CONCRETE FOOTPATHS AS NECESSARY TO INSTALL NEW ROOFWATER PIPE TO BOUNDARY. REINSTATE FOOTPATH WITH N25/20 CONCRETE. REFER TO WESTERN DOWNS REGIONAL COUNCIL STANDARD DRAWING R-010 FOR CONCRETE PATH DETAILS.
7. ALLOW 2 ROOFWATER CONNECTIONS PER PROPERTY. LINE UP KERB ADAPTOR WITH EXISTING HOUSE ROOFWATER PIPE WHERE POSSIBLE. IF NO PIPE EXISTS, INSTALL KERB ADAPTORS AT EACH SIDE OF HOUSE/PROPERTY.

				Field Book No.	DRAWN L. Porter		<div>WESTERN DOWNS REGIONAL COUNCIL</div> 	Horiz. Section	STANDARD DRAWING – ROADS
				Level Book No.	DESIGNED S. Forbes			NOT TO SCALE	ROOFWATER DRAINAGE KERB ADAPTORS
				Datum	CHECKED S. Forbes			Vert. Section	FOR BARRIER KERB AND MOUNTABLE KERB
					EXAMINED G. Parsons				
					RECOMMENDED S. Hegedus RPEQ.				
				TECHNICAL SERVICES MANAGER					
				DATE 19/12/2012					
A	Original Issue				Job No./s	Works Order No.	Auxiliary Plan No's.		Plan No.R-016
Revisions			Drn by	Date					No. 16 of 16Plans
									Rev. A



INTERSECTION TYPE A



INTERSECTION TYPE B

		SIDE ROAD			
		RURAL ACCESS 2	RURAL ACCESS 1	RURAL FEEDER	RURAL COLLECTOR
MINIMUM RADIUS (m)	R1	15			
	R2	10 (SEAL TO TANGENT)	12	15	SWEEP PATH ANALYSIS REQUIRED

		SIDE ROAD			
ROAD HIERARCHY / AADT		RURAL ACCESS 2 (AADT ~0-20)	RURAL ACCESS 1 (AADT ~20-50)	RURAL FEEDER (AADT ~50-100)	RURAL COLLECTOR (AADT ~>100)
THROUGH ROAD	RURAL ACCESS 2 (AADT ~0-20)	AS PER INTERSECTION A			
	RURAL ACCESS 1 (AADT ~20-50)	AS PER INTERSECTION B	AS PER INTERSECTION B		
	RURAL FEEDER (AADT ~50-100)	AS PER INTERSECTION B	INTERSECTION ANALYSIS REQUIRED	INTERSECTION ANALYSIS REQUIRED	
	RURAL COLLECTOR (AADT ~>100)	INTERSECTION ANALYSIS REQUIRED	INTERSECTION ANALYSIS REQUIRED	DESIGN AS PER RPDM CH 13	DESIGN AS PER RPDM CH 13

NOTES:

1. THIS STANDARD IS BASED ON 'NORMAL' SITUATIONS AND USES A 19m SEMI-TRAILER AS A DESIGN VEHICLE. ANY INTERSECTION WHICH IS DEEMED NON-STANDARD OR POTENTIALLY HAZARDOUS WILL REQUIRE ANALYSIS FOR DESIGN. IF THE SIDE ROAD CONTAINS HIGHER THAN 25% COMMERCIAL VEHICLES, OR IS BASED ON DIFFERENT DESIGN VEHICLE, THE INTERSECTION WILL REQUIRE ANALYSIS FOR DESIGN.
2. THE AADT (IF KNOWN) IS TO BE USED AS PREFERENCE OVER THE ROAD HIERARCHY IN TABLE.
3. ROAD TO ROAD TURNING HAS BEEN ALLOWED INTO RURAL ACCESSES 1 & 2, LANE TO ROAD TURNING HAS BEEN ALLOWED INTO RURAL FEEDER. IF THE AADT OF THE THROUGH OR SIDE ROAD IS SIGNIFICANT, TURNING LANE TO LANE SHOULD BE DESIGNED.
4. INTERSECTION ANALYSIS REQUIRES CONSIDERATION OF ALL CIRCUMSTANCES CONCERNING THE INTERSECTION IN DETERMINING WHAT TREATMENT SHALL BE USED. USE OF THE MINIMUM TREATMENT 'INTERSECTION TYPE B' MAY BE ADOPTED IF WARRANTED.
5. THIS STANDARD IS ONLY TO BE USED FOR LEFT AND RIGHT TURNING TREATMENTS INTO RURAL ROADS.
6. THE TURNING PATH SHOULDERS OF 'INTERSECTION TYPE B' SHOULD BE SEALED UNLESS THE SHOULDER CAN BE MAINTAINED WITH A SOUND, EVEN SURFACE.

MINIMUM TURNING TREATMENTS FOR TYPICAL RURAL ROADS

Field Book No.

Level Book No.

Datum

B

Text Changes

S.E.R

24.03.17

A

Original Issue

Revisions

Drn by

Date

DRAWN

DESIGNED

CHECKED

EXAMINED

RECOMMENDED

RPEQ.

DATE

Job No./s

Works Order No.

WESTERN DOWNS

REGIONAL COUNCIL

Auxiliary Plan No's.

Horiz. Section

NOT TO SCALE

Vert. Section

NOT TO SCALE

STANDARD DRAWING – ROADS

MINIMUM TURING TREATMENTS

FOR RURAL ROADS

Plan No.

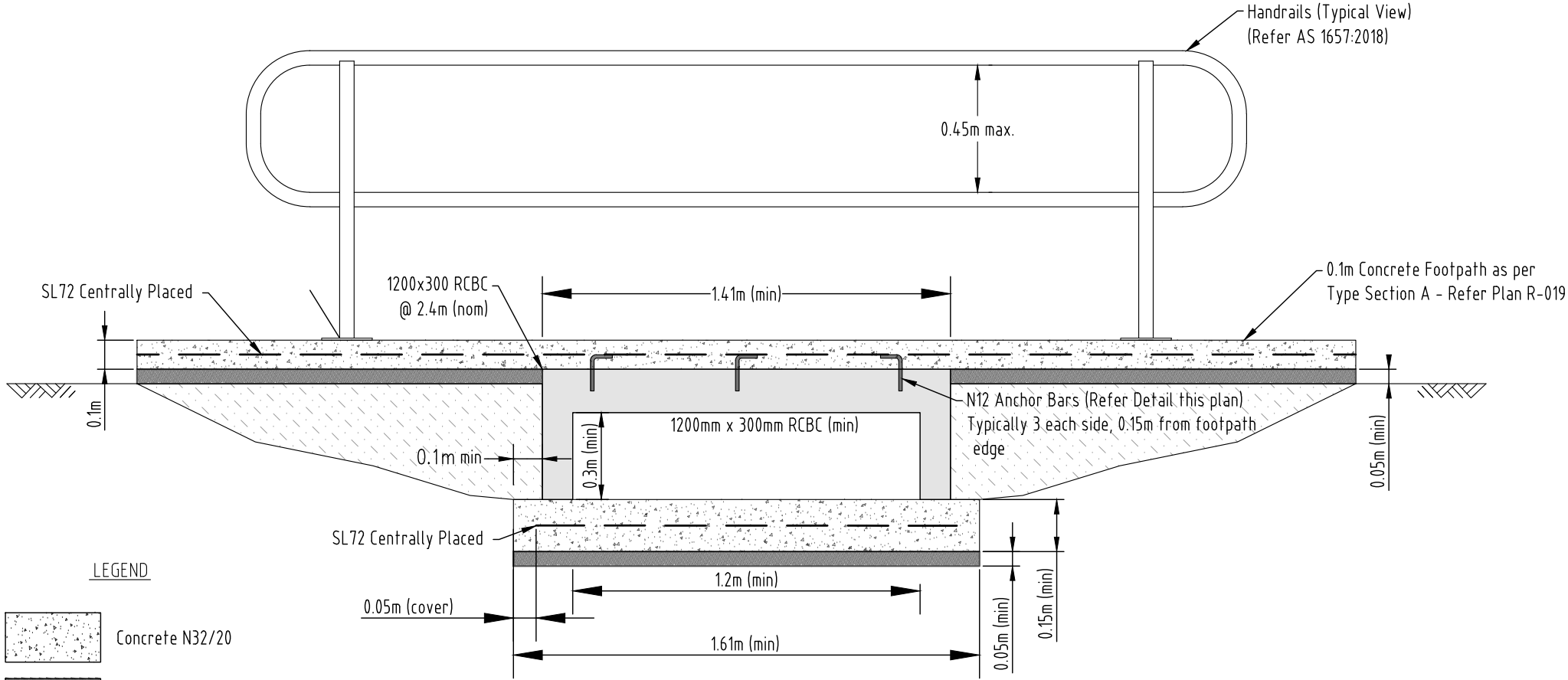
R-017

No. 17 of 17Plans

Rev. B

IPWEA STANDARD DRAWINGS LIST	
STD DRAWING NO.	DESCRIPTION
RS-065	CONCRETE PATHWAYS - CONSTRUCTION DETAILS

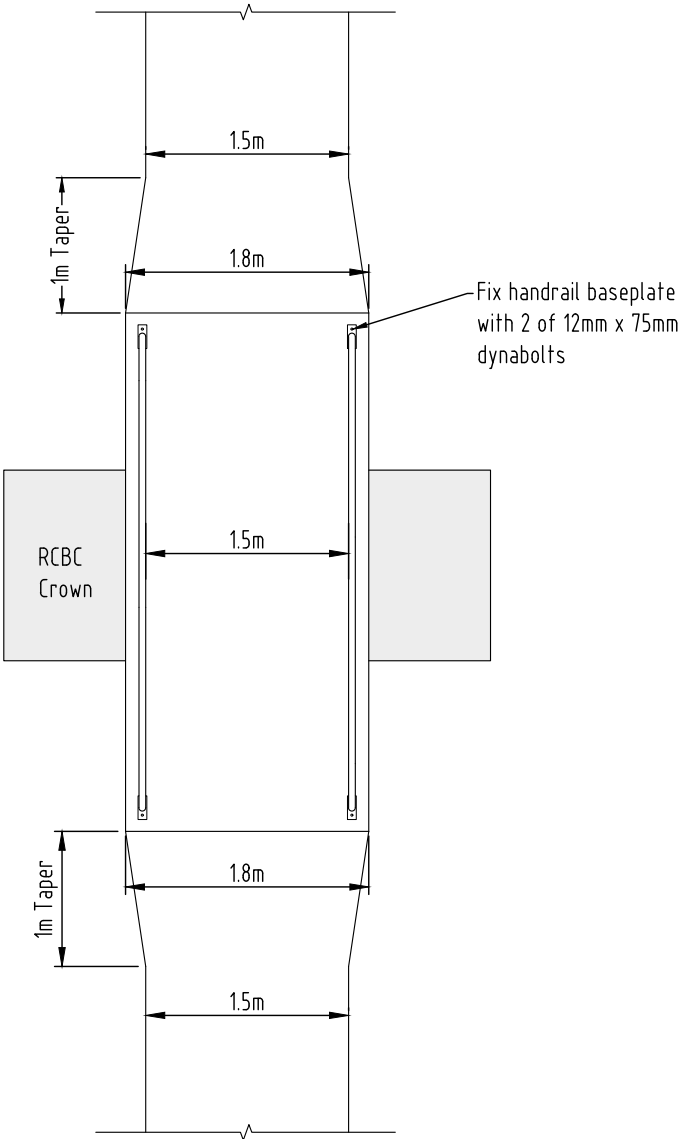
TYPICAL CULVERT & SLAB DETAIL



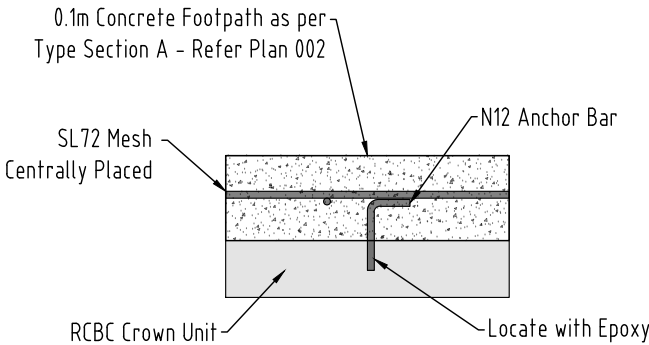
LEGEND

- Concrete N32/20
- Crusher Dust 0.05m (min)
- RCBC 1200mm x 300mm (min)
- Select Material (Refer Project Manager)

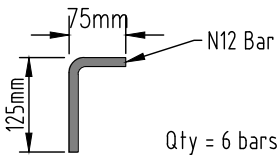
SQUARE CULVERT DETAIL



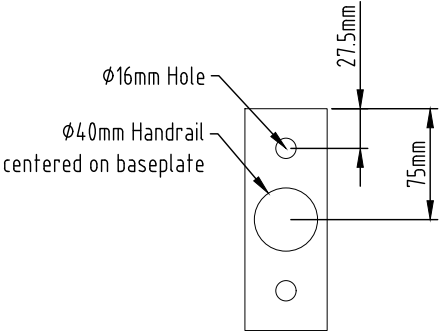
ANCHOR BAR SECTION DETAIL




ANCHOR BAR DETAIL



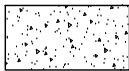
TYPICAL BASE PLATE DETAIL  
(65mm x 8mm flat @ 150mm long)




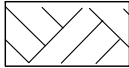
				Field Book No.	DRAWN J.Allen	<div>WESTERN DOWNS REGIONAL COUNCIL</div> <div></div>	Horiz. Section	STANDARD DRAWING – ROADS TYPICAL CULVERT AND SLAB DETAIL
				Level Book No.	DESIGNED J.Allen		NOT TO SCALE	
					CHECKED P.Blissner			
				Datum	EXAMINED		Vert. Section	
					CERTIFIED Certifying Engineer – J.CRICK		NOT TO SCALE	
B	Added Mid–Rail	J.R.A	6/2/23		RPEQ. 7834	<div>Auxiliary Plan No’s.</div>		Plan No. <i>R–018</i> No. 18 of 19Plans      Rev. A
A	Original Issue				DATE 16/02/2023			
Revisions		Drn by	Date					



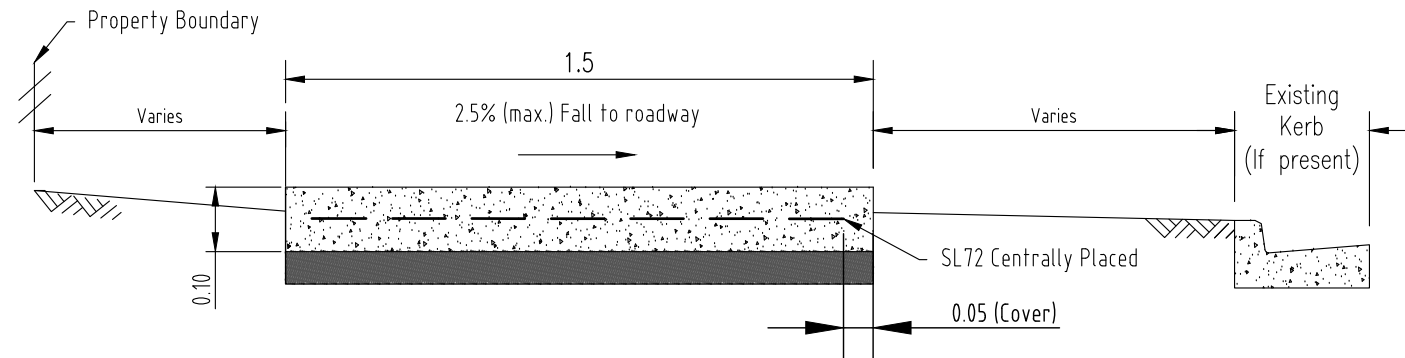
LEGEND

Concrete N32/20

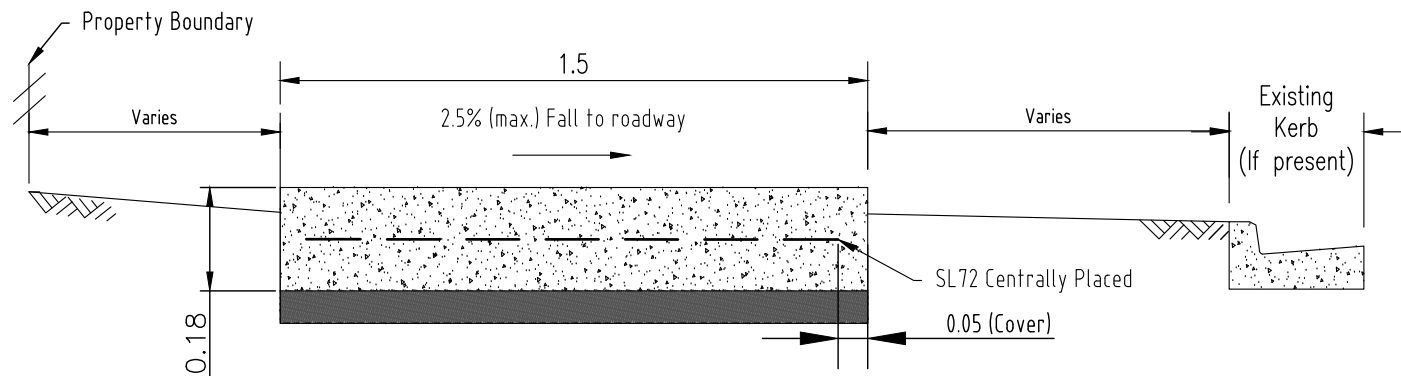
Crusher Dust 0.05m

Natural Fill

TYPICAL TYPE A SECTION DETAIL





TYPICAL TYPE B SECTION DETAIL



IPWEA STANDARD DRAWINGS LIST	
STD DRAWING NO.	DESCRIPTION
RS-065	CONCRETE PATHWAYS - CONSTRUCTION DETAILS
RS-090	KERB RAMPS - RAMPED PEDESTRIAN CROSSINGS
RS-091	KERB RAMPS - CUT THROUGH TREATMENTS
RS-092	KERB RAMPS - INSTALLATION OF 'TGSi's' (1 of 2)
RS-093	KERB RAMPS - INSTALLATION OF 'TGSi's' (2 of 2)
RS-094	KERB RAMPS - LOCATIONS and CONFIGURATIONS

FOOTPATH CONSTRUCTION

1. Excavate existing surface to subgrade level, add bedding material as required
2. 'Box-up' and construct footpath as per required 'Typical Cross Section' shown this plan
3. Join smoothly to existing concrete works with required joints (Confirmation by Project Manager)  
Refer standard drawing table this plan
4. Drainage needs to be maintained to ensure water is free flowing across footpath and overland  
flow does not bank up inside property boundary.
5. Construct Pram Ramps (w/- TGSi's) if required. Refer standard drawing table this plan
6. Refer Table this Plan for a list of IPWEA Standard Drawings as referred to above
7. Constructor to liaise with Project Manager for all construction queries

				Field Book No.	DRAWN J.Allen		STANDARD DRAWING – ROADS TYPICAL CONCRETE FOOTPATH DETAILS	
				Level Book No.	DESIGNED J.Allen			
					CHECKED P.Blissner			
				Datum	EXAMINED			
					CERTIFIED Certifying Engineer – J.Crick			
					RPEQ. 7834			
A	Original Issue					DATE 15/10/2020		
Revisions		Drn by	Date			Auxiliary Plan No's.	Plan No.R-019	No. 19 of 19Plans Rev. A